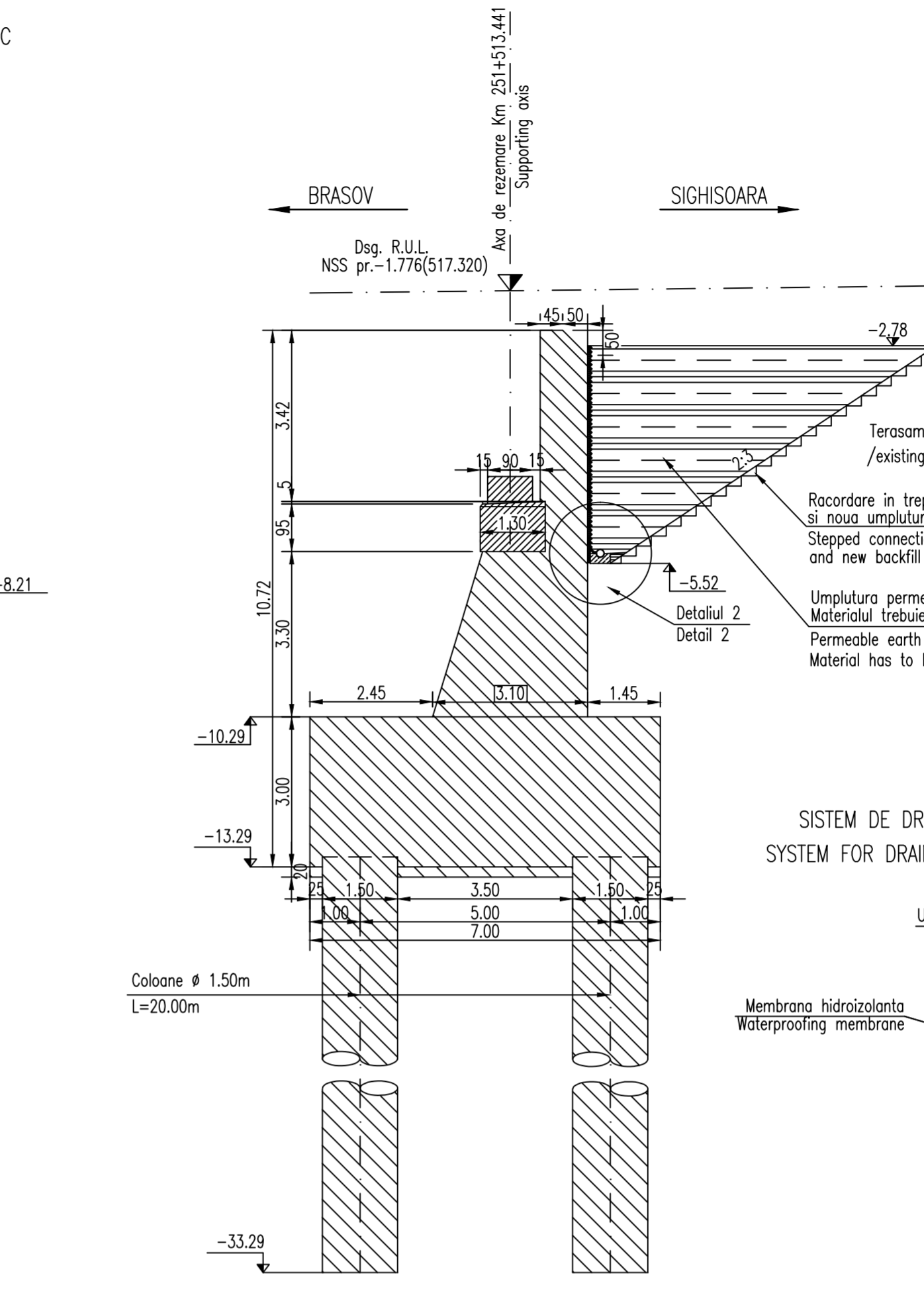
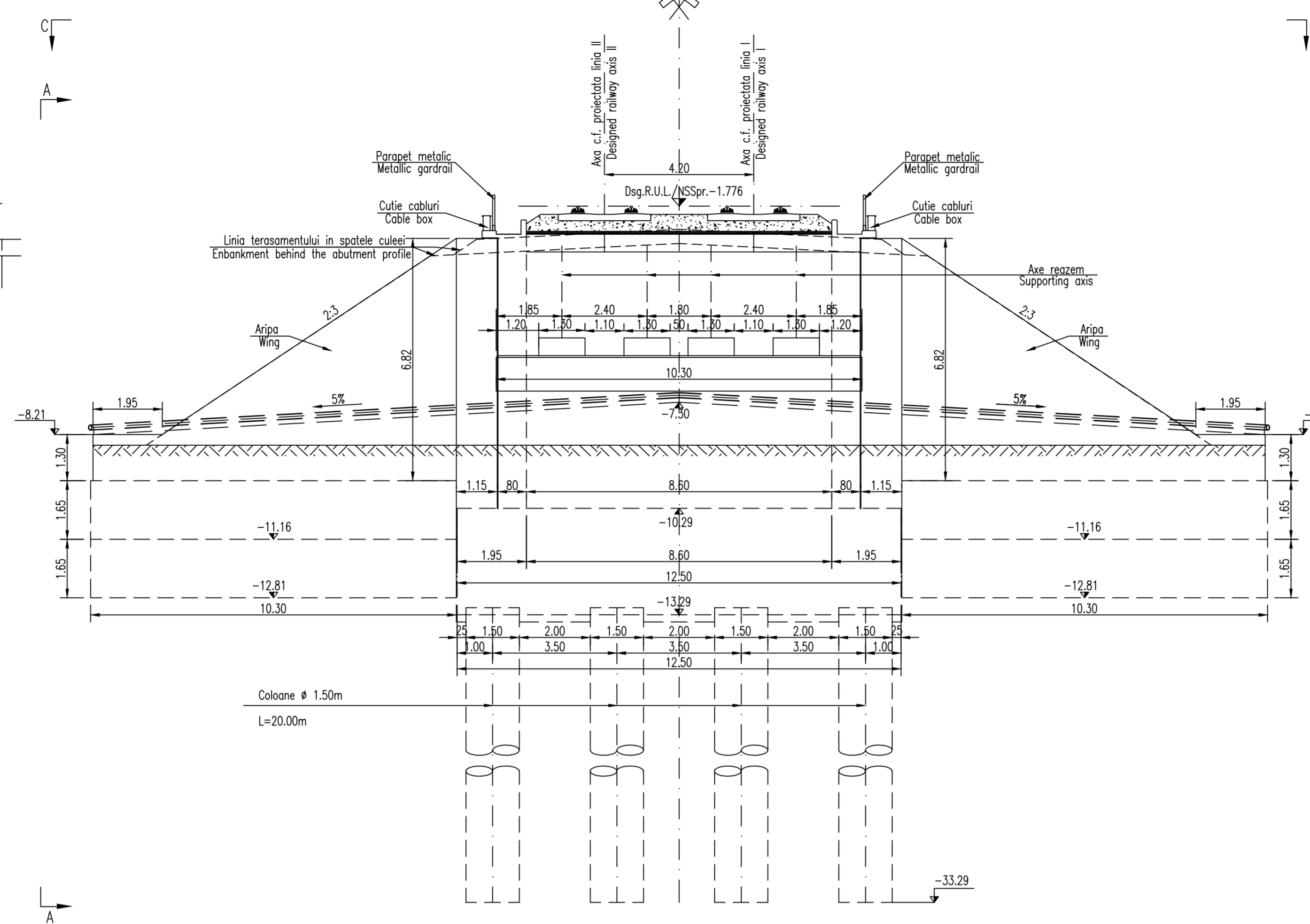
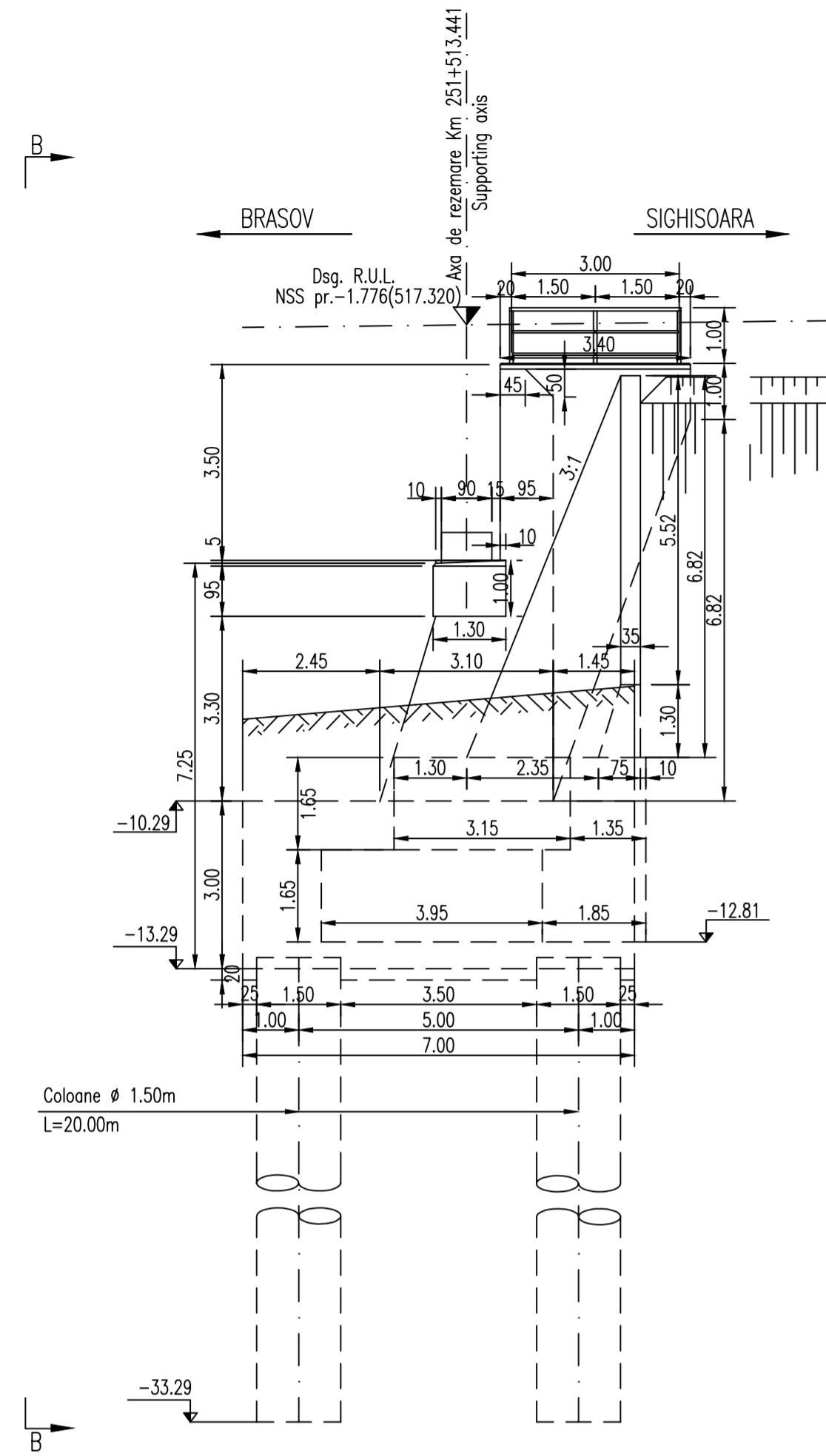


ELEVATIE A-A
ELEVATION A-A
Scara 1:100

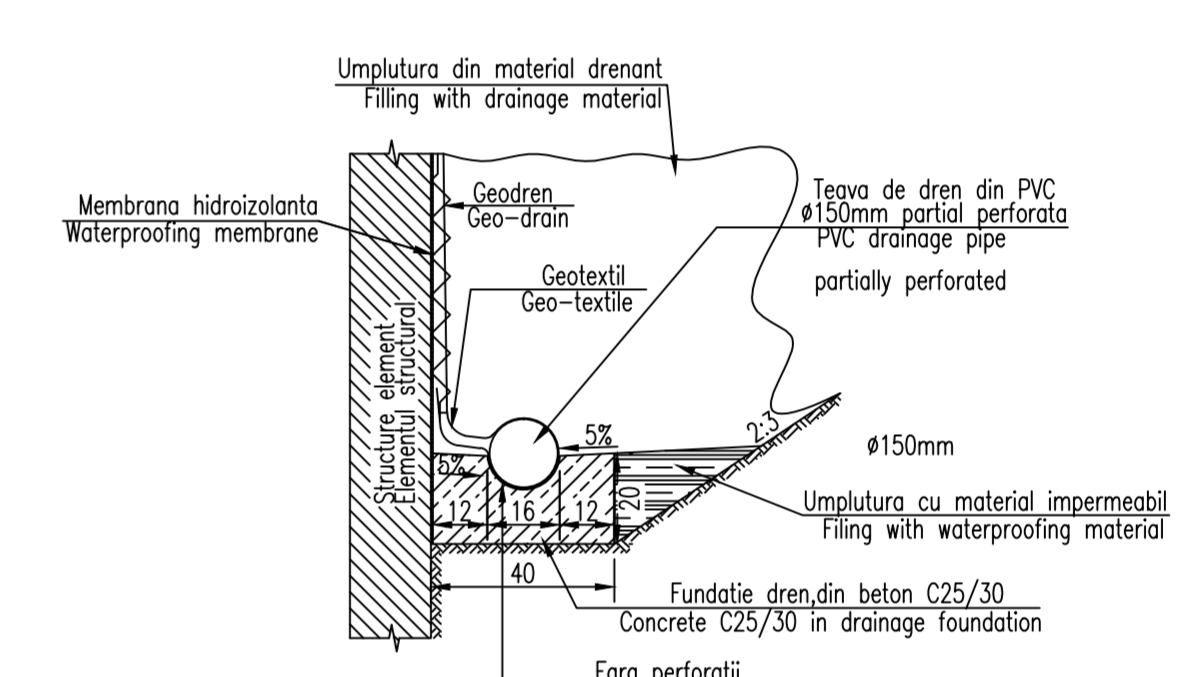
VEDERE FRONTALA B-B
FRONT VIEW B-B
Scara 1:100

SECTIUNE TRANSVERSALA D-D
CROSS SECTION D-D
Scara 1:100

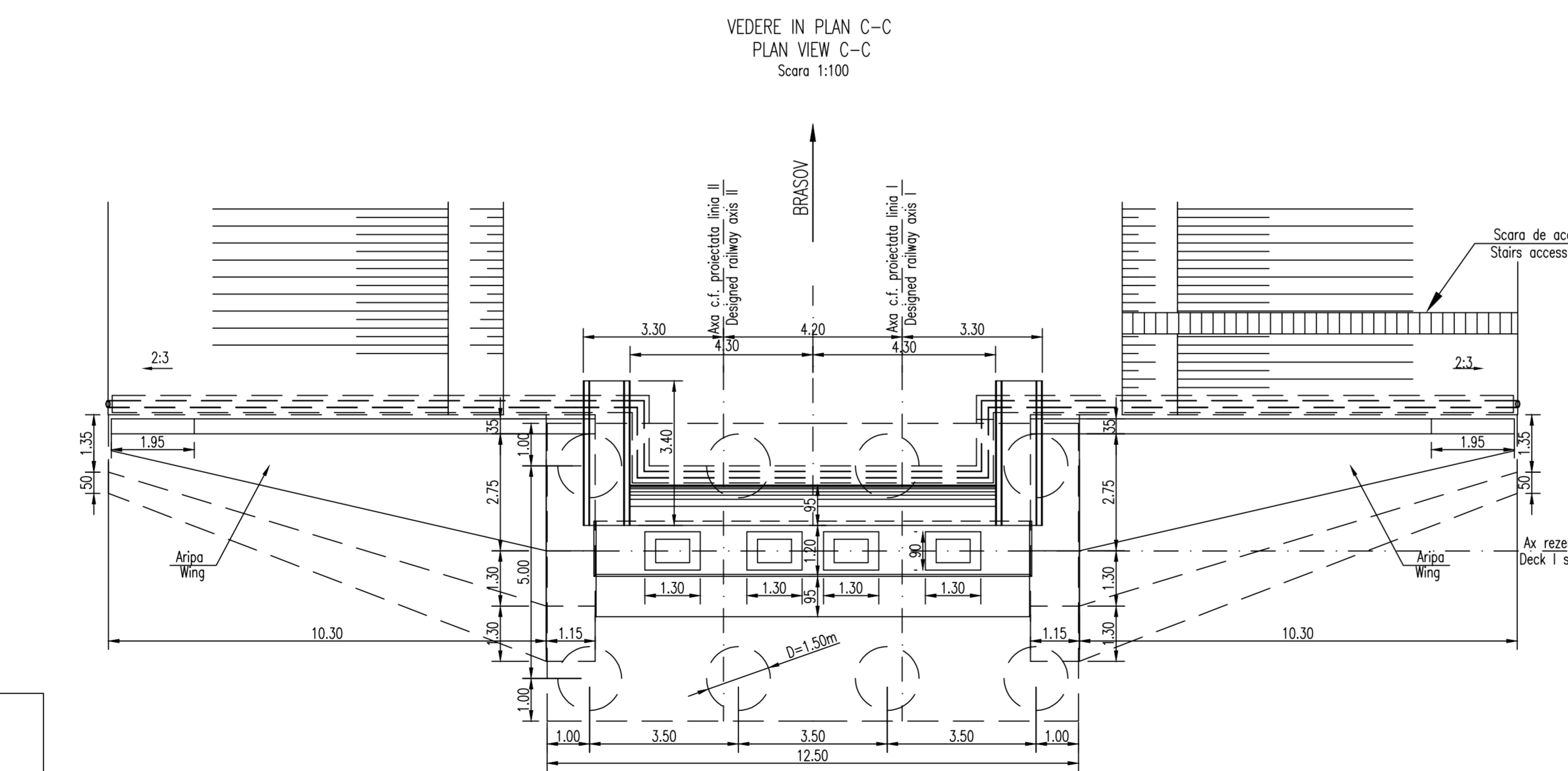


Terasament existent/
/existing embankment
 Racordare in trepte intre terasamentul existent
 si noua umplutura din spatele podului
 Stepped connection between existing embankment
 and new backfill to bridge abutment
 Umplutura permeabila de pamant Gradul de compactare Dp=100%
 Materialul trebuie asezat si compactat in straturi de max. 30 cm
 Permeable earth filling with compaction degree Dp=100%
 Material has to be laid out and be compacted in layers of maximal 30 cm

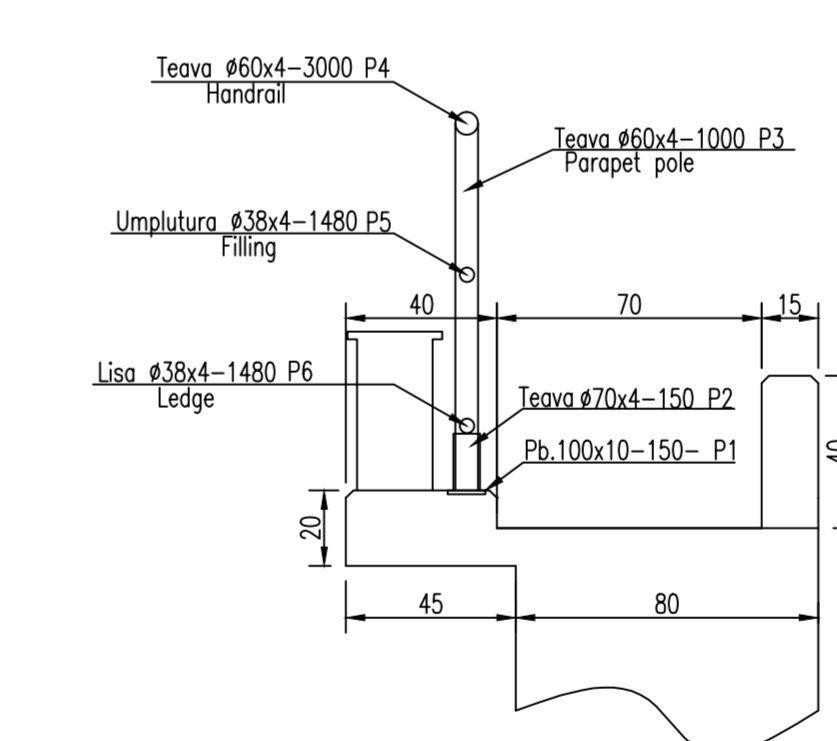
DETALIU "2"/DETAIL "2"
SISTEM DE DRENARE A APELOR DIN SPATELE CULEELOR
SYSTEM FOR DRAINAGE THE WATER FROM BEHIND THE ABUTMENTS



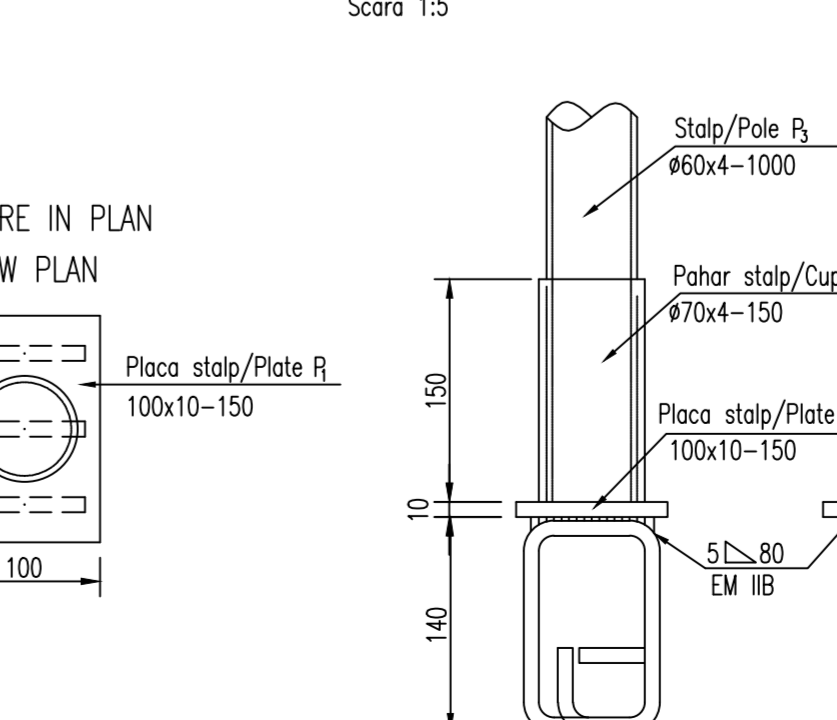
VEDERE IN PLAN C-C
PLAN VIEW C-C
Scara 1:100



DETALIU TROTUAR/SIDEWALK
Sc 1:20



DETALIU 1 DE PRINDERE A PARAPETULUI
1 DETAIL HANDRAIL FASTENING
Scara 1:5



NOTA:
 1. Prezentul plan s-a intocmit in conformitate cu dispozitia generala.
 2. Podul corespunde la convosiile de calcul UIC(LM 71 si SW/2).
 3. La executie se vor respecta cu strictete prevederile din "Normativ pentru producerea betonului si executarea lucrarilor din beton, beton armat si beton precomprimat. Partea 1: Producerea betonului", indicativ NE 012/1-2007 si "Normativ pentru producerea betonului si executarea lucrarilor din beton, beton armat si beton precomprimat. Partea 2: Executarea lucrarilor din beton", indicativ NE 012/2-2010, iar verificarea calitatii lucrarilor si receptionarea lor se va face conform normativului C56-1985.
 4. Constructia se incadreaza in categoria de importanta B (constructii de importanta deosebita), modelul 1-de asigurare a calitatii si clasa de importanta B, conform HG 766/1997.
 5. Proiectul va fi verificat la exigentele A4.2; B2.2; D2.2.

NOTE:
 1. This plan has been drawn up according to the general disposition.
 2. The bridge corresponds to the UIC determination convoys (LM71 and SW/2).
 3. The execution will strictly comply with the provisions of "Practice code for the concrete production and works execution, reinforced and pre-stressed concrete - Part 1: Practice code for concrete production" NE 012/1-2007 and "Practice code for the concrete production and works execution, reinforced and pre-stressed concrete - Part 2: The execution of concrete works." NE 012/2-2010, and the quality and the reception of the works shall be made accordingly to Norm C 56-1985.
 4. The construction has been classified in B category of importance (high important constructions), model 1-ensuring the quality and class of importance B according to G.O. 766/97.
 5. The project will be checked in order to comply with the A4.2; B2.2; D2.2 requirements.

LEGENDA BETOANELOR/CONCRETE LIST

	Beton armat in cuzineti si in bancheta cuzinetilor Reinforced concrete in the cushions and cushions bench C 35/45-CEM I/A-S 52,5-(XC4+XF3+XA2)-A/C=0,50-0,32-CI 0,20
	Beton armat in fundatii indirecte(coaloane si radieri) si in elevatii Plain concrete in abutments foundations: C 25/30-CEM II/A-S 42,5-(XC4+XF3)-A/C=0,50-0,32-CI 0,20
	Beton simplu in fundatiile si elevatiile aripilor Simple concrete in the wings foundations and elevations C 25/30-CEM II/A-S 32,5-(XFA)-A/C=0,50-0,32-CI 0,20
	Beton de egalizare Concrete in the protection layer of the waterproofing C 25/30-CEM II/A-S 32,5-(XF3)-A/C=0,55-0,16-CI 0,20 _{max}

In cazul in care temperatura in timpul turnarii este scazuta, se vor folosi cimenturile cu rezistenta initiala mare, R si aditivi acceleratori, iar in cazul turnarii pe timp cald, cimenturile cu rezistenta initiala uzuala, N si aditivi intarziatori (conf.NE 012/2-2010 si tabelului 2 din SR EN 197-1:2002)
 When the temperature during the casting is low, cements with high initial resistance, R and accelerating additives shall be used and when it is cast during warm weather, cements with common initial resistance, N and delaying additives shall be used (according to the norm NE 012/2-2010 and table 2 for the SR EN 197-1: 2002).

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Cotele sunt raportate la NSS ±0.00(519.096) din dreptul axei de rezemare a culeei Brasov(Km 251+359.041).
 Benchmarks are reported to RUL ±0.00(519.096)next to the supporting axis of the abutment Brasov(Km 251+359.041).

Aprovizionarea aparatelor de rezemare se va face inaintea executarii cuzinetilor si a banchetei ocolitoare. In cazul in care inaltimea aparatelor de rezemare difera fata de cea considerata in proiect se vor modifica inaltimile pe rezemare corespunzator cu noile aparate.
 The bearing blocks shall be supplied before building the bearings and their seats.In case the height of the supplied bearing blocks differs from that given in the project , the heights on the seats shall be adjusted so as to comply with the new devices.

D					
C					
B					
A	12.2011	Reviz 1 1 Revision	Totan Ciprian		
Index	Date	Modificari Modification/Revision	Proiectant Designer	Aprobant Consultant Approved Consultant	Aprobant CFR Approved CFR



GUVERNUL ROMANIEI
ROMANIAN GOVERNMENT
 PROIECT FINANAT DE UNIUNEA EUROPEANA
EUROPEAN UNION FINANCED PROJECT
 C.N.C.F. "C.F.R." - S.A.



CONSULTANT / CONSULTANT		Data	Semnatura
Aprobat Approved	Set proiect Project manager		
Aprobat Approved	Coordonator Sectiune 1 Section 1 Coordinator		
Verificat Checked	Expert Cheie Checking Expert		

SUBCONTRACTANT / SUBCONTRACTOR		Data	Semnatura
Aprobat Approved	Responsabil Subcontractant Subcontractant Responsible		
Intocmit Elaborated	Proiectant Designer		

Reabilitarea liniei de cale ferata Brasov - Simeria, parte componenta a coridorului IV Pan European, pentru circulatia trenurilor cu viteza maxima de 160 km/h,
 Tronsoanel: Brasov - Sighisoara
 Rehabilitation of the railway line Brasov - Simeria, component Part of the IV Pan-European Corridor, for the trains circulation with maximum speed of 160 km/h,
 Section : Brasov - Sighisoara

Denumire desen / Drawing Title :
 INTERVAL / SECTION CATA - ARCHITA
 Viaduct Km 251+436.241 D=5x30.00m
 PLAN COFRAJ CULEE SIGHISOARA/DECKING PLAN ABUTMENT SIGHISOARA

Codificare / Codification System	Scara / Scale 1:100	LOT / LOT	Nr. / No 01 / 01
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E A 5 1 0 1 E 1 4 B B P V 0 2 9 2 0 0 5 1