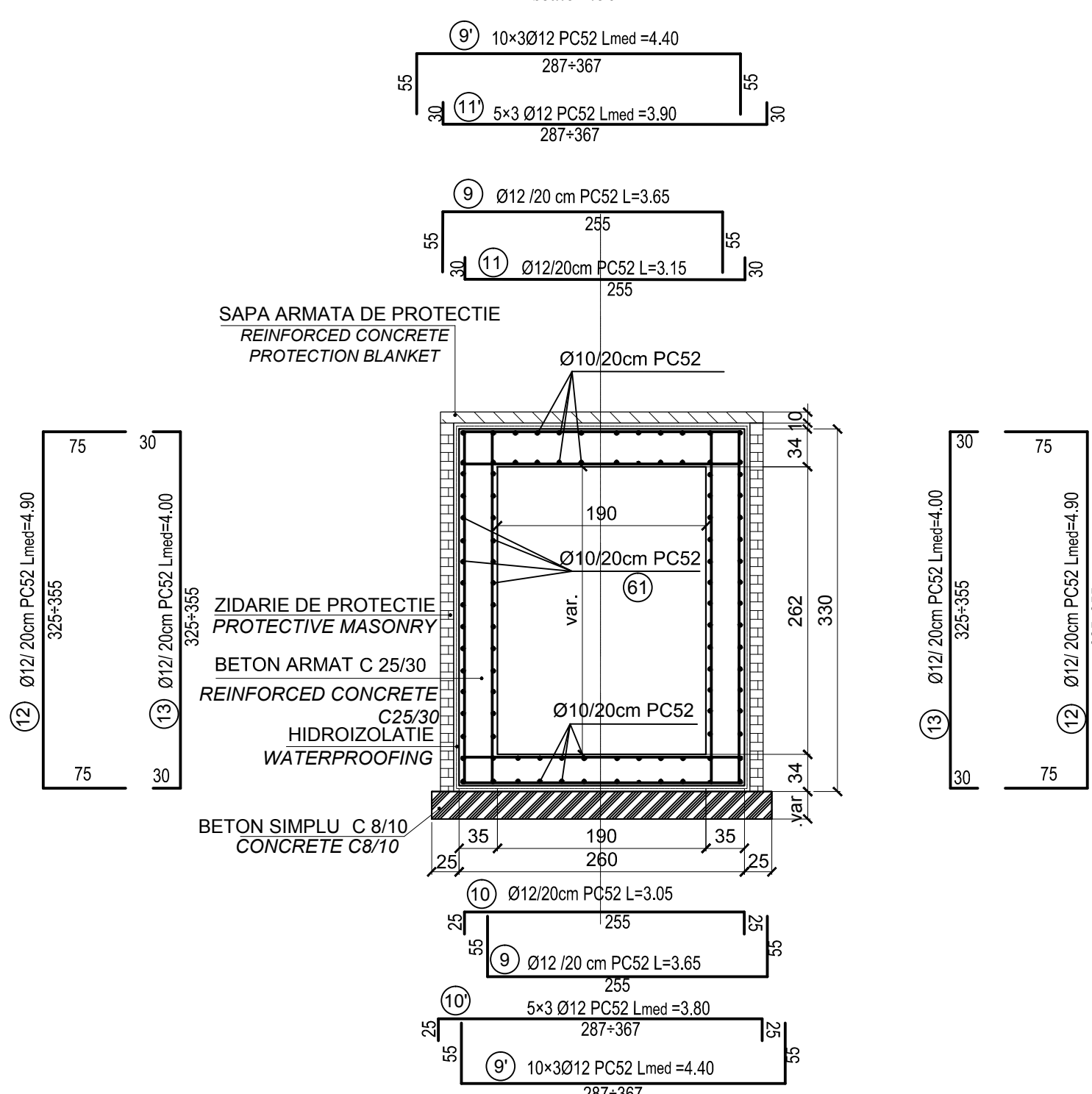


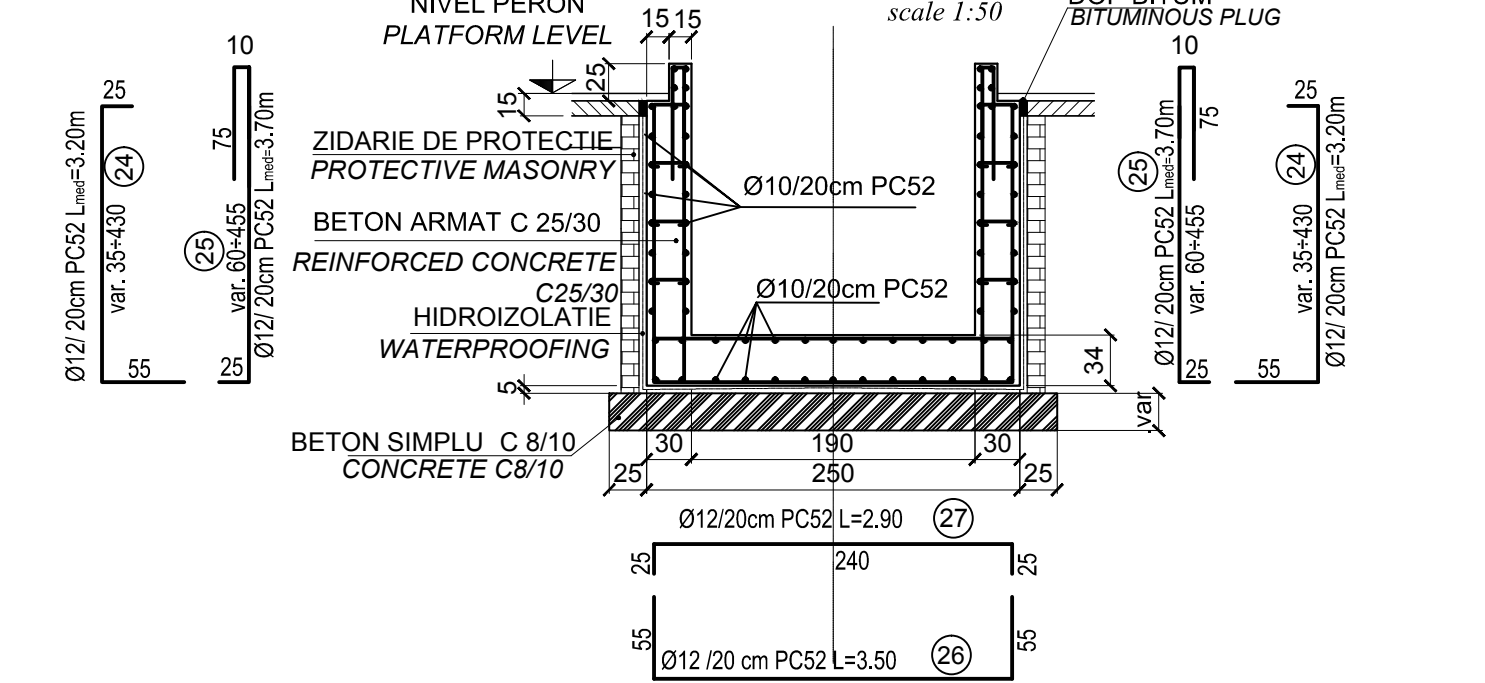
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SECTION : F-F

scara 1:50
scale 1:50



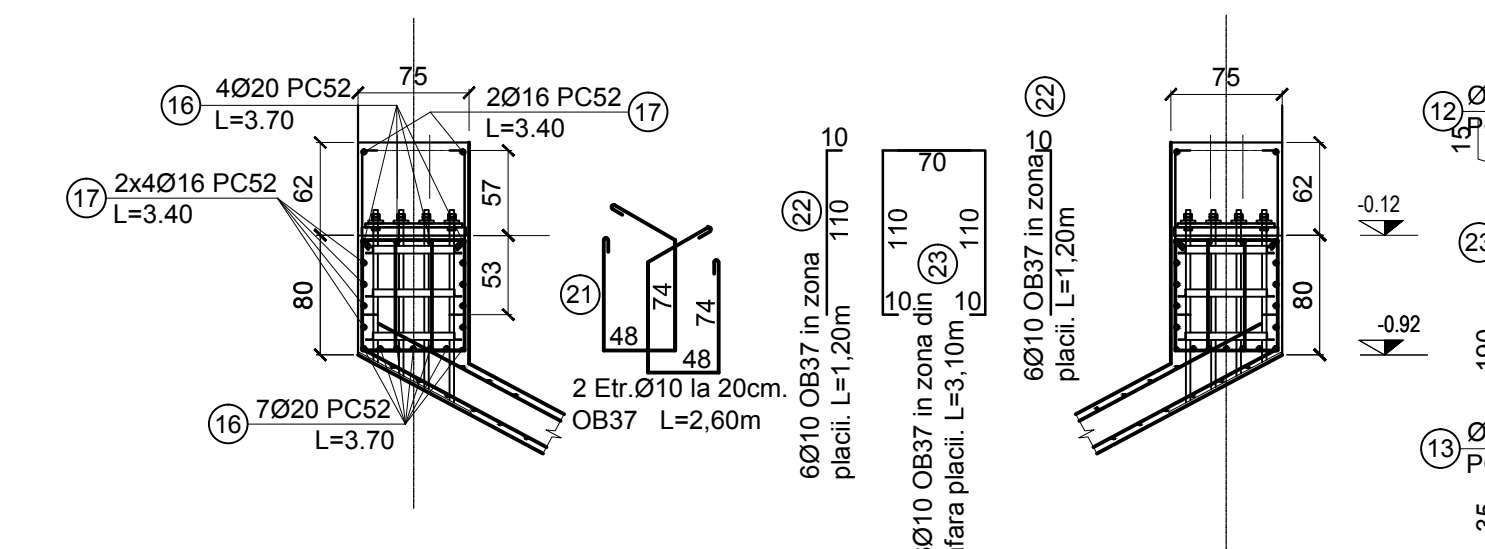
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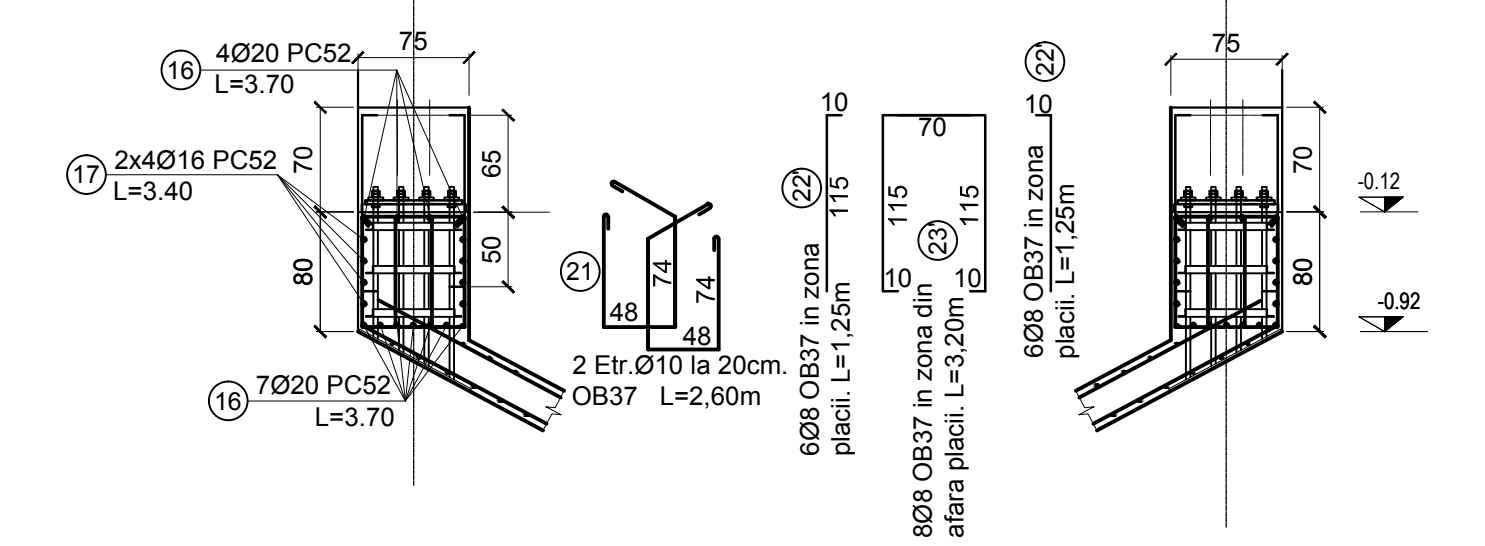


DETALII ARMARE GRINDA PE TUNEL

PENTRU SECTIUNEA C - C

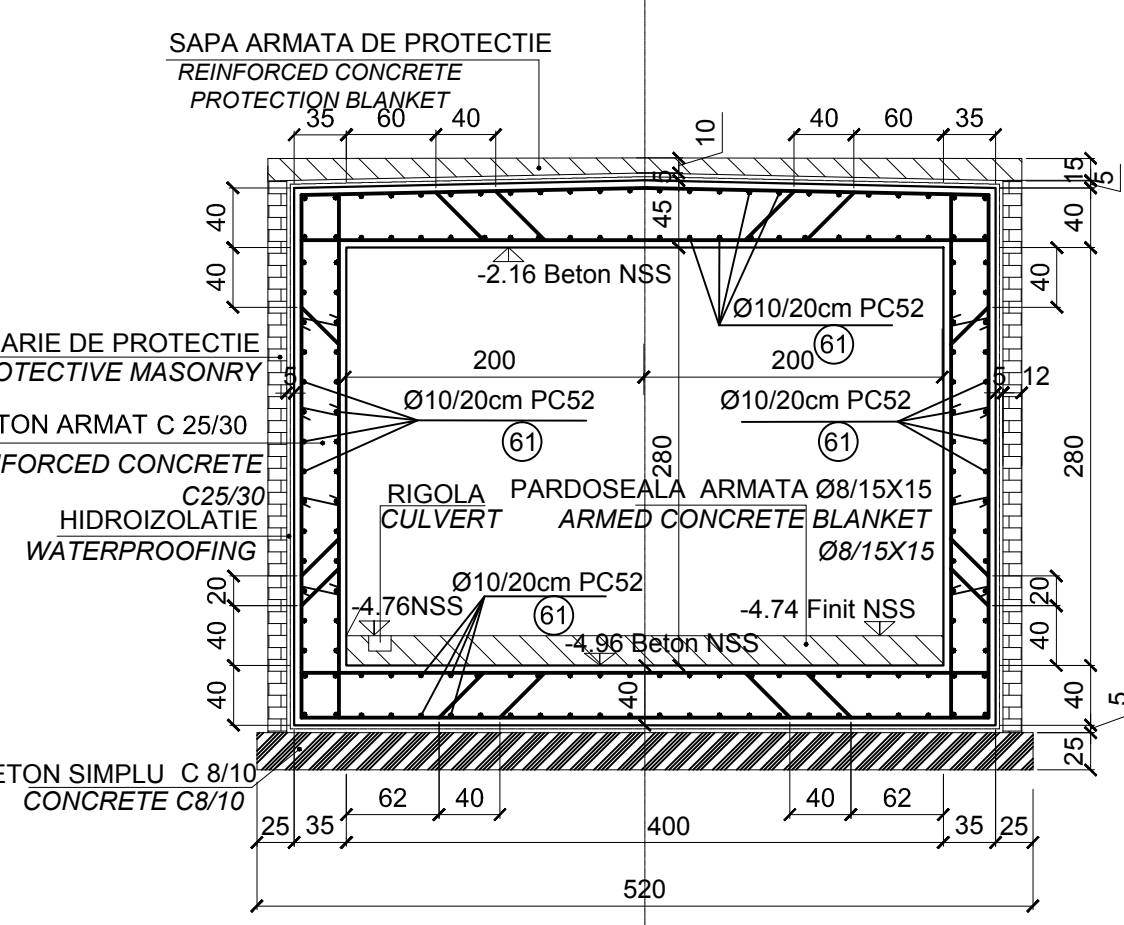
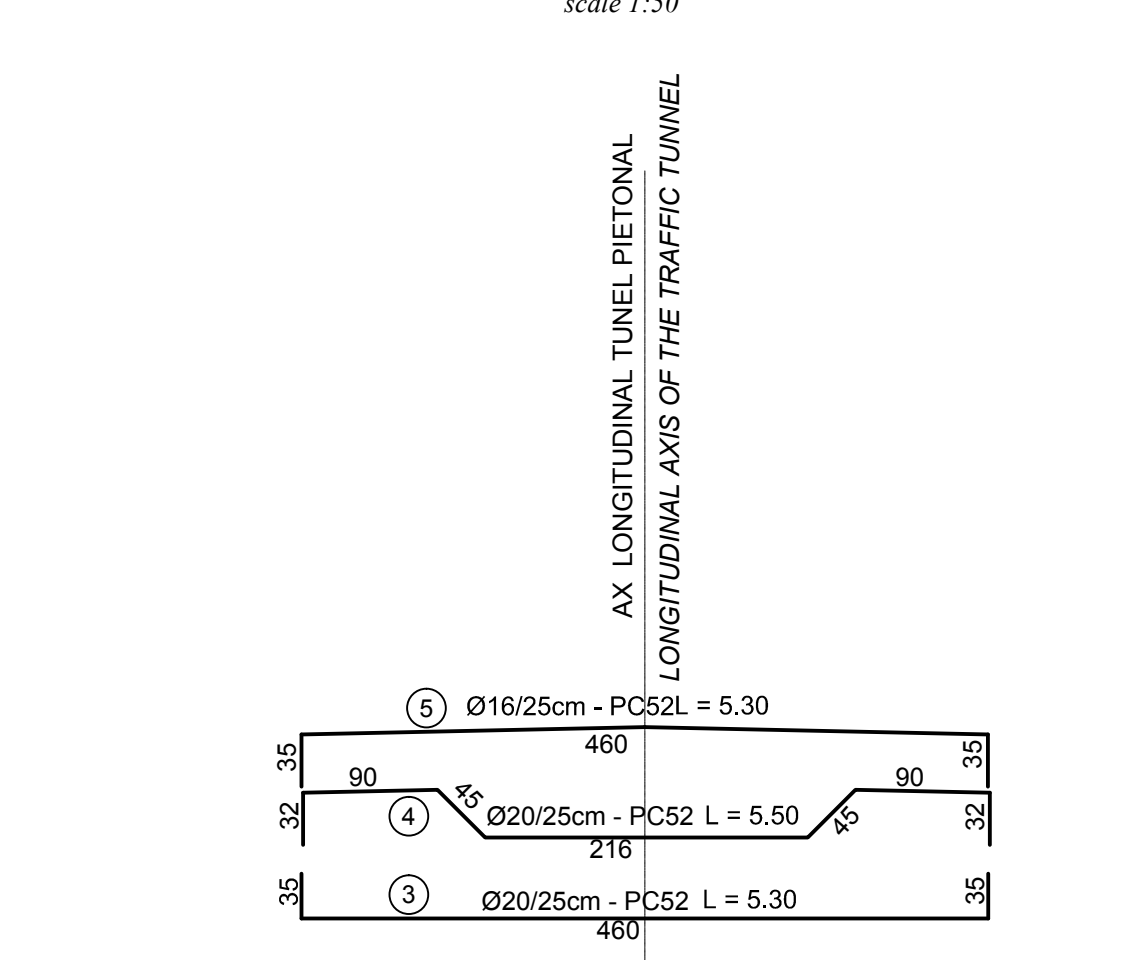


PENTRU SECTIUNEA B - B si D - D



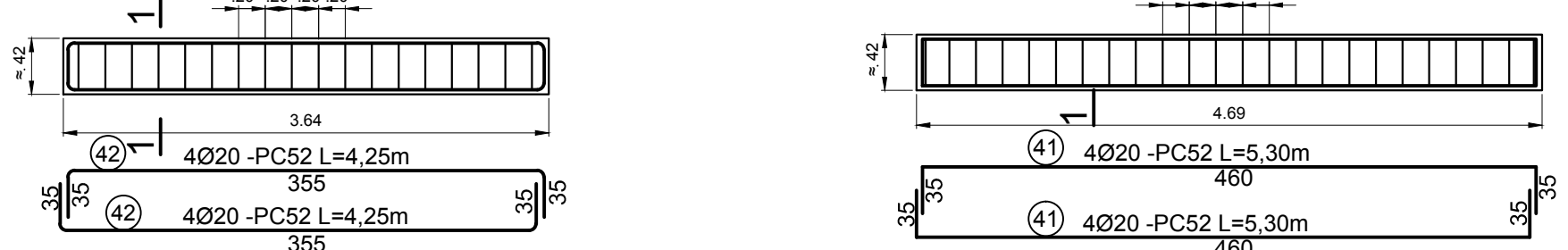
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scara 1:50
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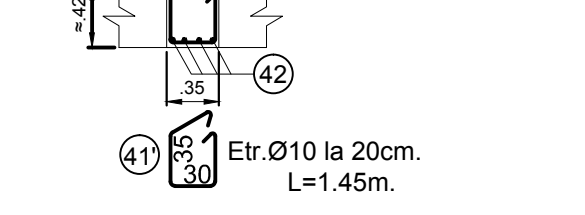
ARMARE GRINDA G1 - 35x42 -2buc. ARMARE GRINDA G2 - 35x42 -2buc.

SCARA 1:50



SECTIUNEA 1 - 1

SCARA 1:50



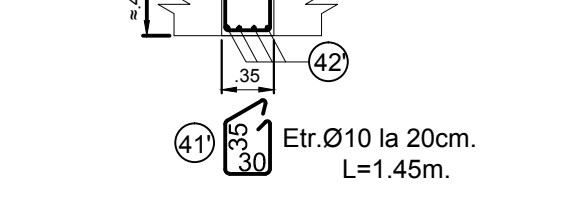
ARMARE GRINDA G3 - 35x42 -1buc.

SCARA 1:50



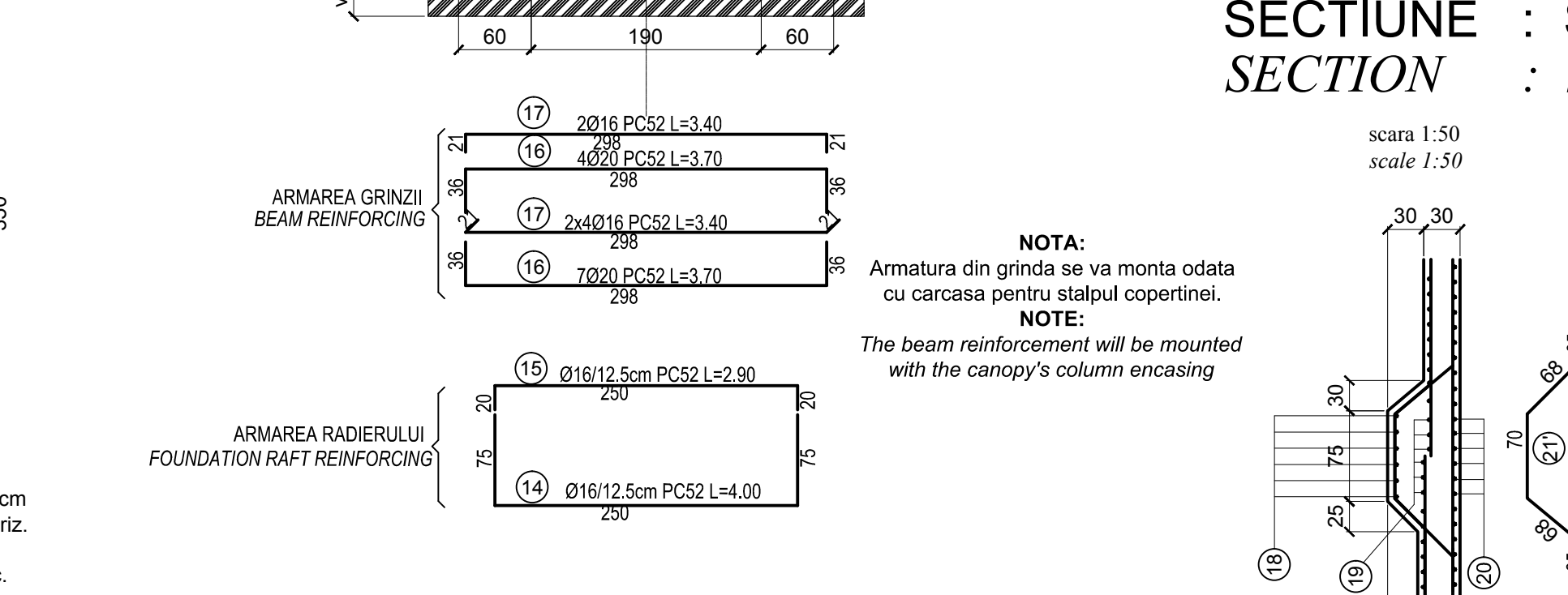
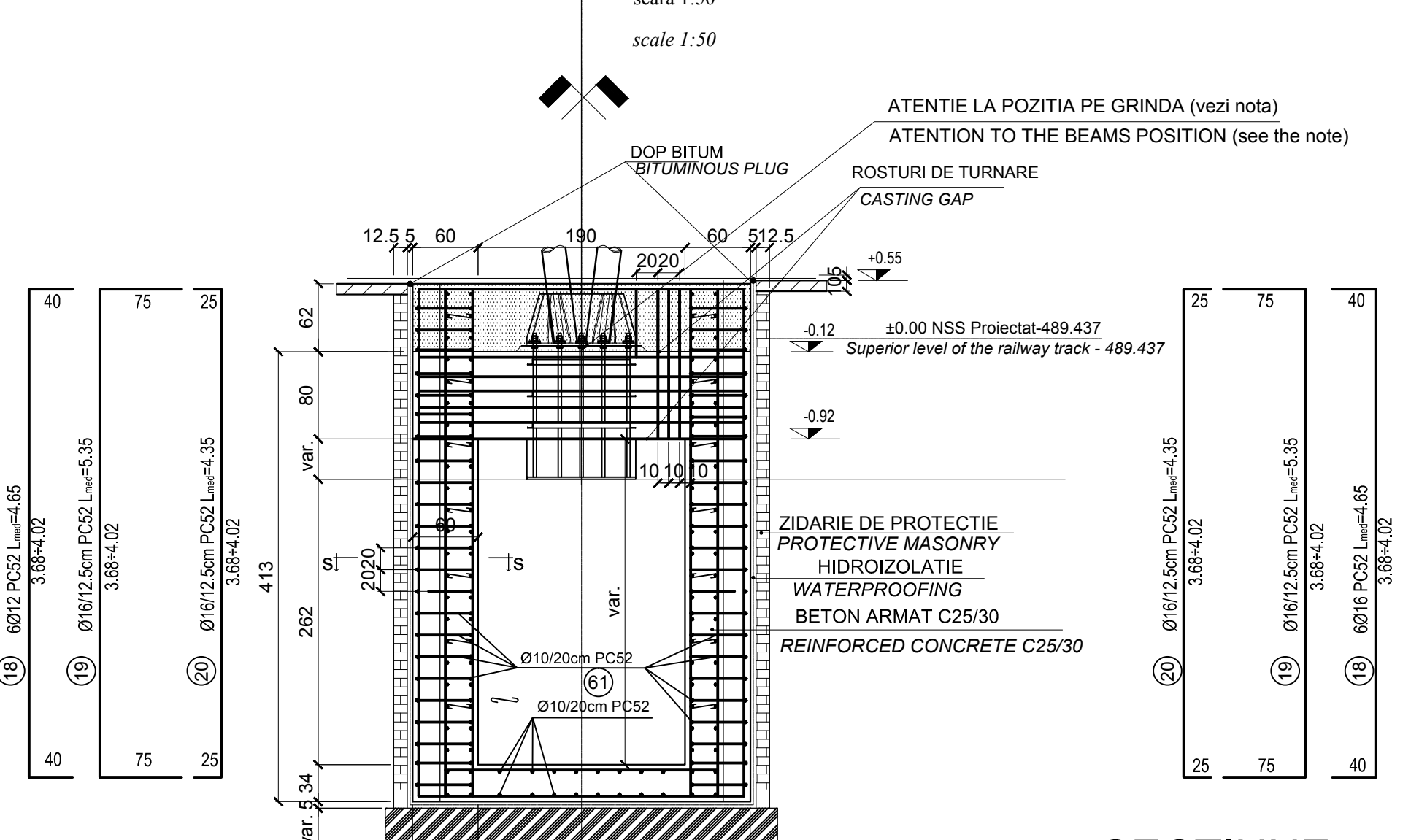
SECTIUNEA 1 - 1

SCARA 1:50



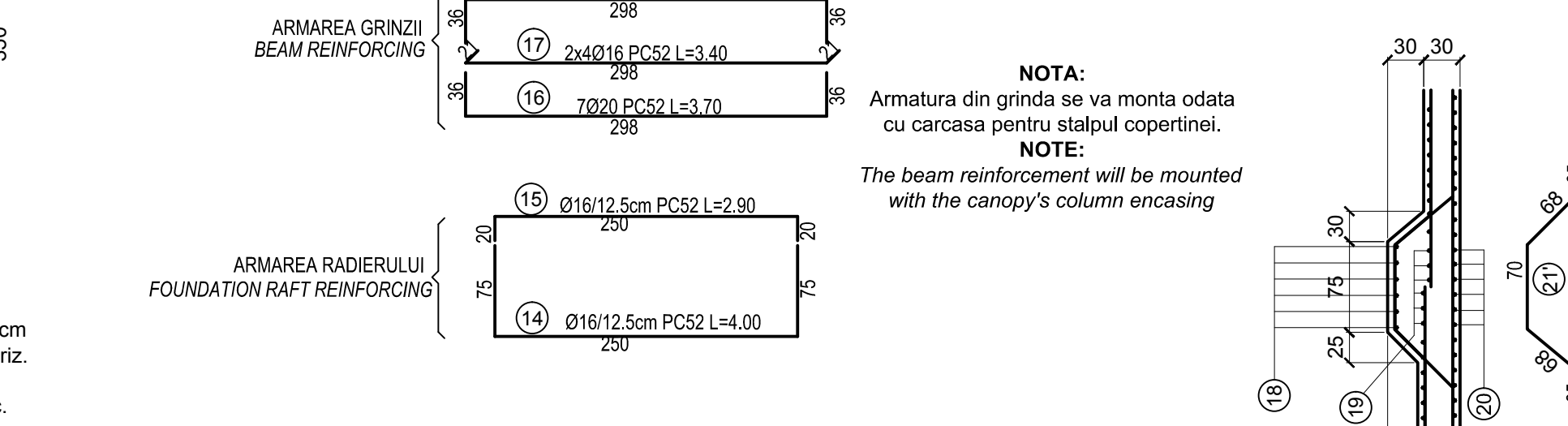
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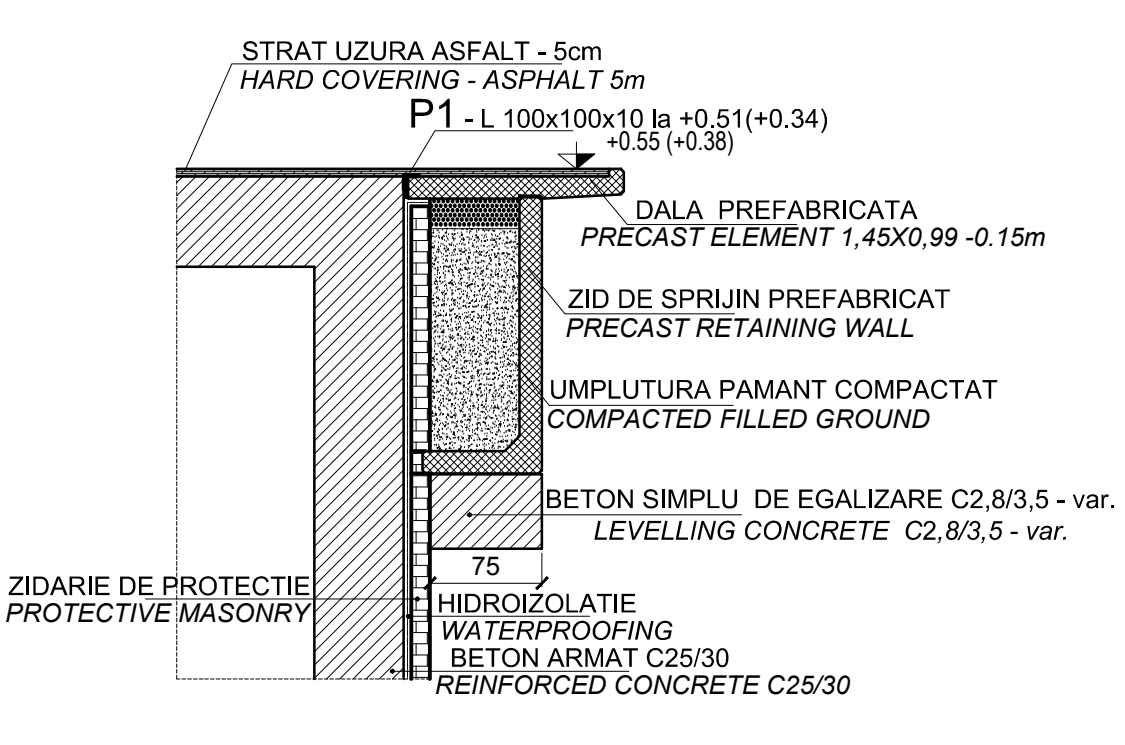


SECTIUNE : S-S
SECTION : S-S

scara 1:50
scale 1:50



DETALII MONTARE DALA PREFABRICATA IN ZONA GRINDA FUNDATIE COPERTINA



EXTRAS LAMINATE									
Pozitia elementului	Denumirea elementului	Grosime (mm)	Latime (mm)	Lungime (mm)	Greutate/ml (kg/ml)	Tip Beton	Nr. Buc	Gr. Pozitie	Gr. Totala (kg)
P1	Tabla prosa	10	100	750	7.85	CL52	10	5.888	58.875
TOTAL GREUTATE SUBSANSIBILE NETIPIZATE (kg)									58.875
GREUTATE ELECTROZI DE SUDURA 3% (kg)									1.766
TOTAL GENERAL(kg)									60.641

PENTRU EXTRASUL DE ARMATURA VEZI PLAN TUNEL

Beton simplu Plain concrete - C8/10 - P4 T3 - IIA - (32.5 (R))
Beton armat Reinforcing concrete - C25/30 - P4 T3 - IIA - (42.5 (R))
Armături Reinforcements - PC 52
Armături Reinforcements - OB 37

RECOMANDARI TEHNOLOGICE SPECIFICE PENTRU FUNDATII STALPI COPERTINE PE TUNEL

Pentru pozitionarea carcasei cu buloanele de ancoraj in grinda tunelului se vor realiza urmatoarele:
In plan orizontal:
Se materializeaza axa copertinei (din dreptul grinzii), pe cofraj.
Se introduce carcasa in cofraj si se asigura verticalitatea buloanelor pe cele doua directii rectangulare.
Se aliniaza axa carcasei cu axa copertinei, prin deplasarea carcasei in lungul grinzii.
OBSERVATIE 1: Axa longitudinala a copertinei (trasata intodeauna fata de axele c.f. ale liniilor adiacente), nu coincide cu axa scarii de acces in tunel, sau axa peronului (cazul copertinelor in curba). Axa carcasei se aliniaza INTOTDEAUNA cu axa copertinei (in consecinta, axa stalpului copertinei nu va mai fi la jumateea lungimii grinzii tunelului).
In plan vertical:
Cota superioara a grinzii tunelului (C.S.G.) trebuie sa fie la -0.12 m fata de N.S.S. proiectat (din dreptul grinzii); vezi : Sectiuni longitudinale prin scari acces tunel.
OBSERVATIE 2: In statiile in care niveleta este orizontala, C.S.G.stanga este identica cu C.S.G.dreapta. In statiile in care niveleta are panta ascendenta sau descendenta (dupa caz), C.S.G.stanga difera de C.S.G.dreapta, dar sunt intodeauna la -0.12 m fata de N.S.S. proiectat (conf. punctului 2 - recomandari tehnologice generale) **Armare, betonare:**
Se armeaza grinda.
Partea superioara a carcasei buloanelor de ancoraj se va asigura obligatoriu intr-un tipar pentru a se evita deplasarea accidentala la turnarea betonului.
Se betoneaza grinda.

SPECIFIC TECHNICAL RECOMMENDATIONS FOR CANOPY FOUNDATION ABOVE TUNNEL

To place the the anchoring bolts case in the tunnel beam, the following will be carried out:
Horizontal alignment:
The axis of the canopy (nearby the beam) will be marked on the shuttering.
The case will be introduced into the shuttering and the verticality of bolts will be provided on the two rectangular directions.
The case axis will be lined up with the canopy axis, by moving the case along the beam.
REMARK 1: The longitudinal axis of the canopy (always traced in relation to the railway axis of adjacent lines) does not coincide to the axis of tunnel access stairs or the platform axis (case of canopies in curve). In this situation, the case axis will be ALWAYS lined up with the canopy axis (consequently, the canopy pole axis will not be placed at half of the length of tunnel beam).
Vertical level alignment:
The tunnel beam upper level (C.S.G.) shall be placed at -0.12 m from the designed rail upper level, R.U.L. (nearby the beam); see Longitudinal sections through the tunnel access stairs.
REMARK 2: In the stations where the level is horizontal, C.S.G left is identical to C.S.G right. In the stations where the level follows the upward / downward slope (depending on the case), C.S.G left is different from C.S.G right, but they are always placed at -0.12 m from the designed rail upper level R.U.L.; (conf. conditions mentioning at point 2 - general technological recommendations).
Reinforcing, concreting:
The beam will be reinforced (according to the Tunnel beams reinforcing details).
The upper part of the anchoring bolt case will be compulsorily secured with a frame to avoid accidental shifting during concrete casting.
The beam will be concreted.

Index	Date	Modification/Revision	Projectant Designer	Approved Consultant	Approved CFR
A					

GUVERNUL ROMANIEI ROMANIAN GOVERNMENT
PROIECT FINANTAT DE UNIUNEA EUROPEANA EUROPEAN UNION FINANCED PROJECT

CFR
C.N.C.F. "C.F.R." - S.A.

CLIENT / CLIENT
ITALFERR
Scaun Wilson
OBERMEYER
PLANEX + BERKEL COOL
TECNIC
Consulting Engineers

CONSULTANT / CONSULTANT		Date	Semnatura
Approved	Project manager	R. Lukaz	12.2011
Approved	Coordinator Section 1	C. Gambelli	12.2011
Verified	Verifier	Giuseppe Fioravanti	12.2011

SUBCONTRACTANT / SUBCONTRACTOR		Date	Semnatura
Approved	Responsible Subcontractant	A. Stancu - Dinulescu	12.2011
Approved	Subcontractant Representative		
Involved	Projector	Catalin Alexandrescu	12.2011

Reabilitarea liniei de cale ferata Brasov - Simeria, parte componenta a coridorului IV Pan European, pentru circulatia trenurilor cu viteza maxima de 160 km/h.
Transonul : Brasov - Sighisoara
Rehabilitation of the railway line Brasov - Simeria, component Part of the IV Pan-European Corridor, for the trains circulation with maximum speed of 160 km/h.
Section : Brasov - Sighisoara

Denumire desen / Drawing Title :
STATIA FELDIOARA TUNEL PIETONAL DETALII ARMARE SECTIUNI TRANSVERSALE
E-E; F-F; F'-F'; G-G-I-I; DETALII ARMARE GRINZI PE TUNEL -
FELDIOARA STATION PEDESTRIAN TUNNEL REINFORCEMENT DETAILS CROSS
SECTIONS E-E; F-F; F'-F'; G-G-I-I. TUNNEL BEAMS REINFORCEMENT DETAILS

Codificare / Codification System	Scara / Scale	LOT/LOT	Nr. / No
EAS1	1:50	01	01