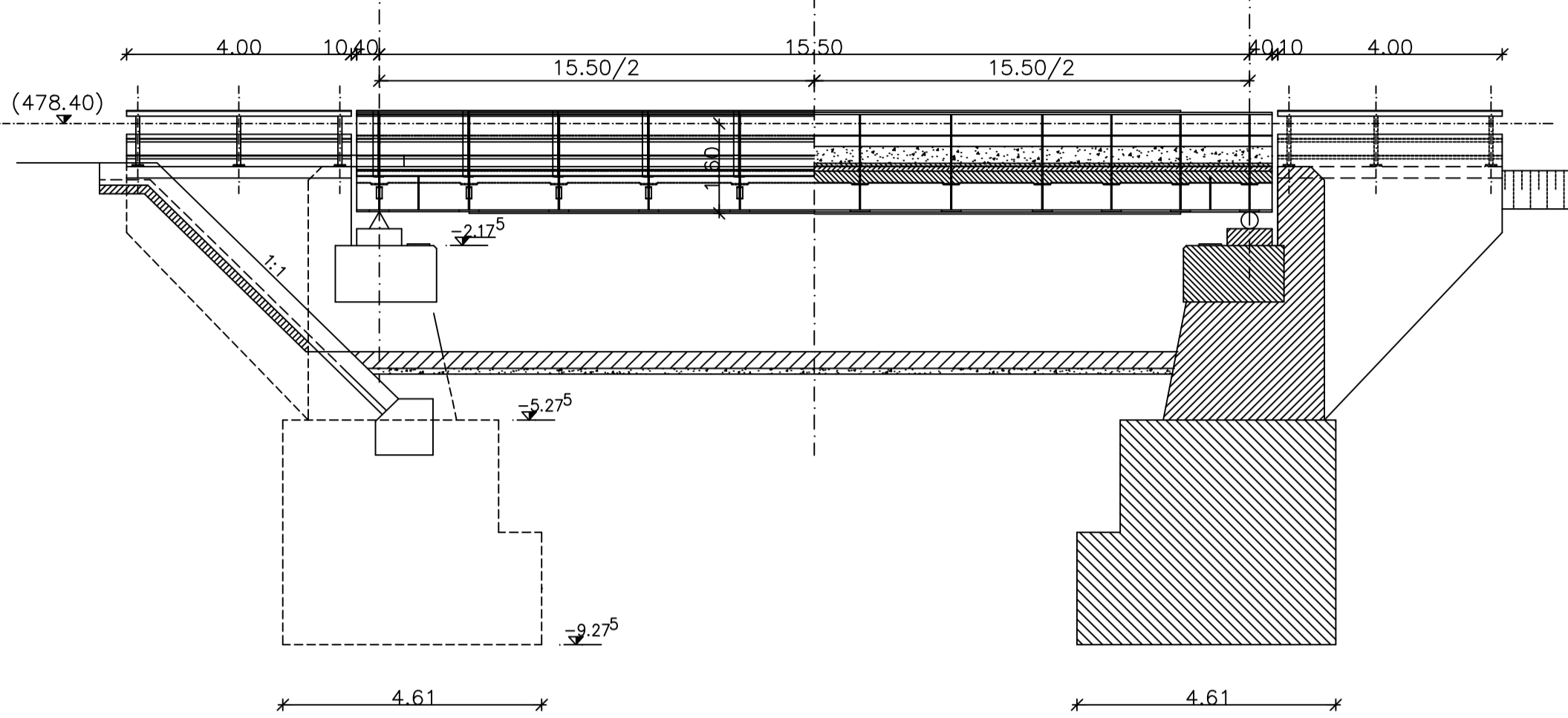
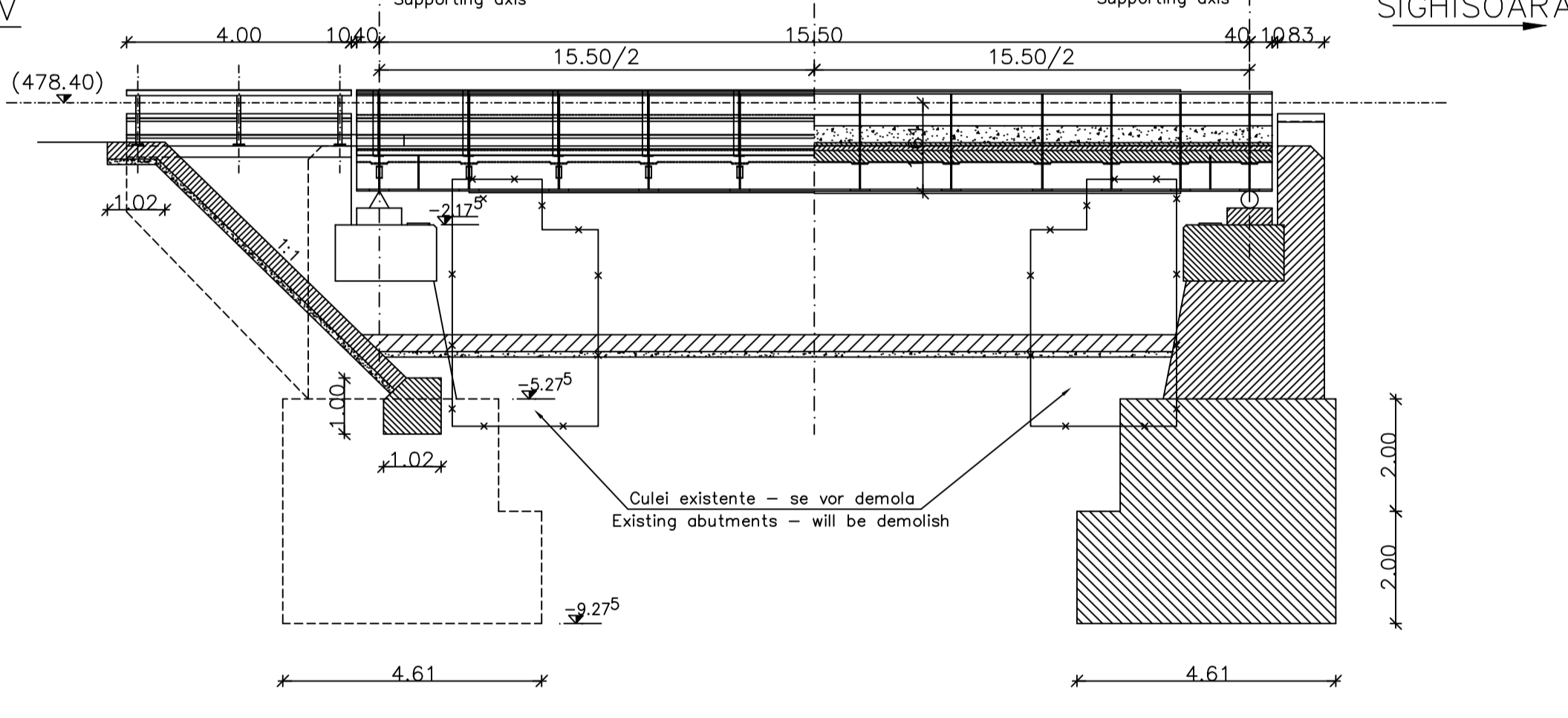


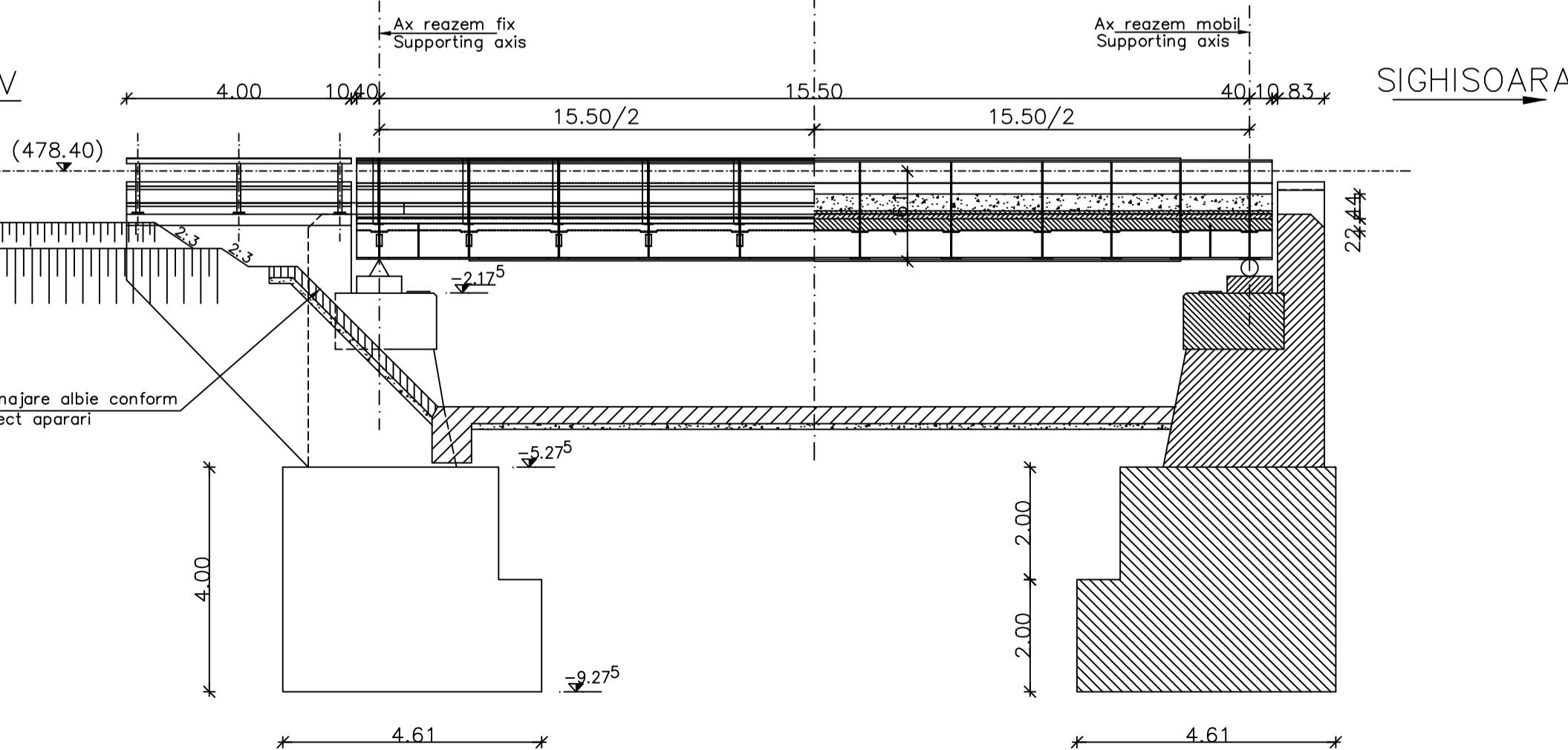
SECTIUNE E-E/SECTION E-E | SECTIUNE F-F/ SECTION F-F



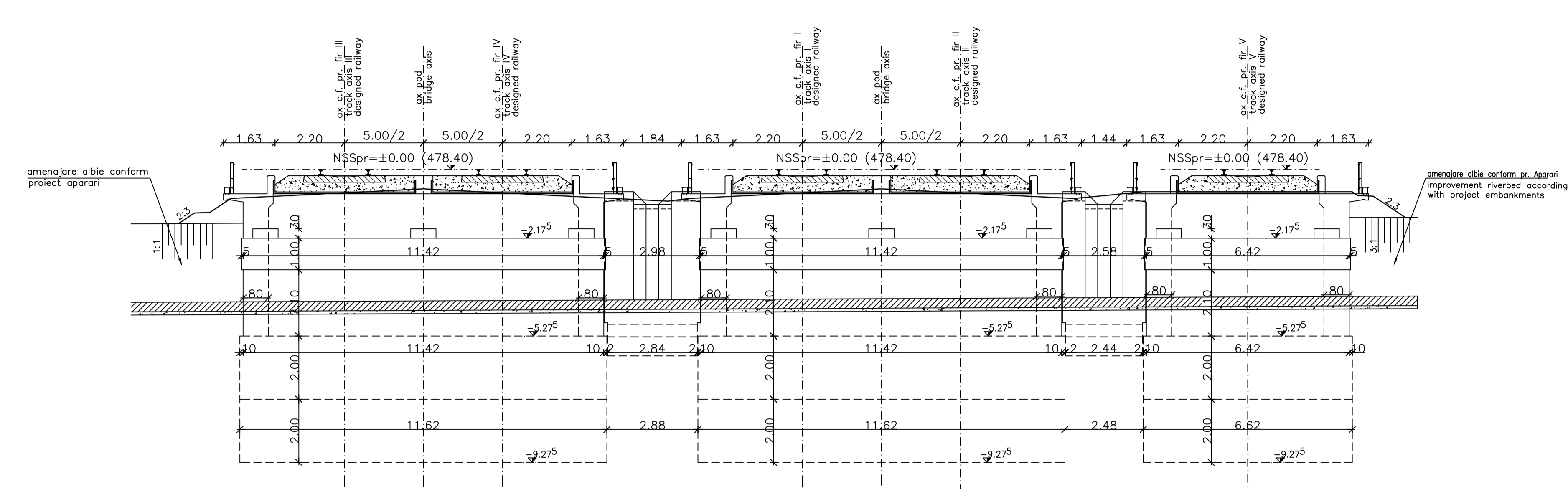
SECTIUNE C-C/SECTION C-C | SECTIUNE D-D/ SECTION D-D



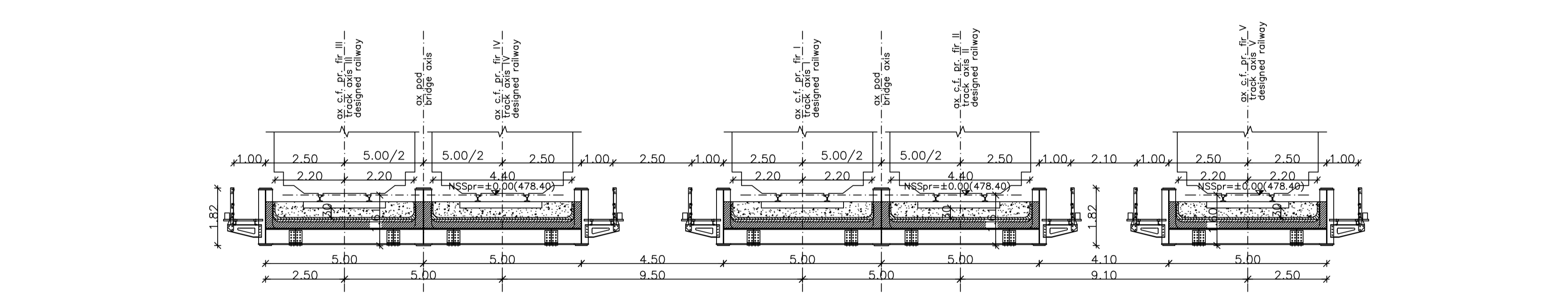
ELEVATIE A-A/ELEVATION A-A | SECTIUNE B-B/ SECTION B-B



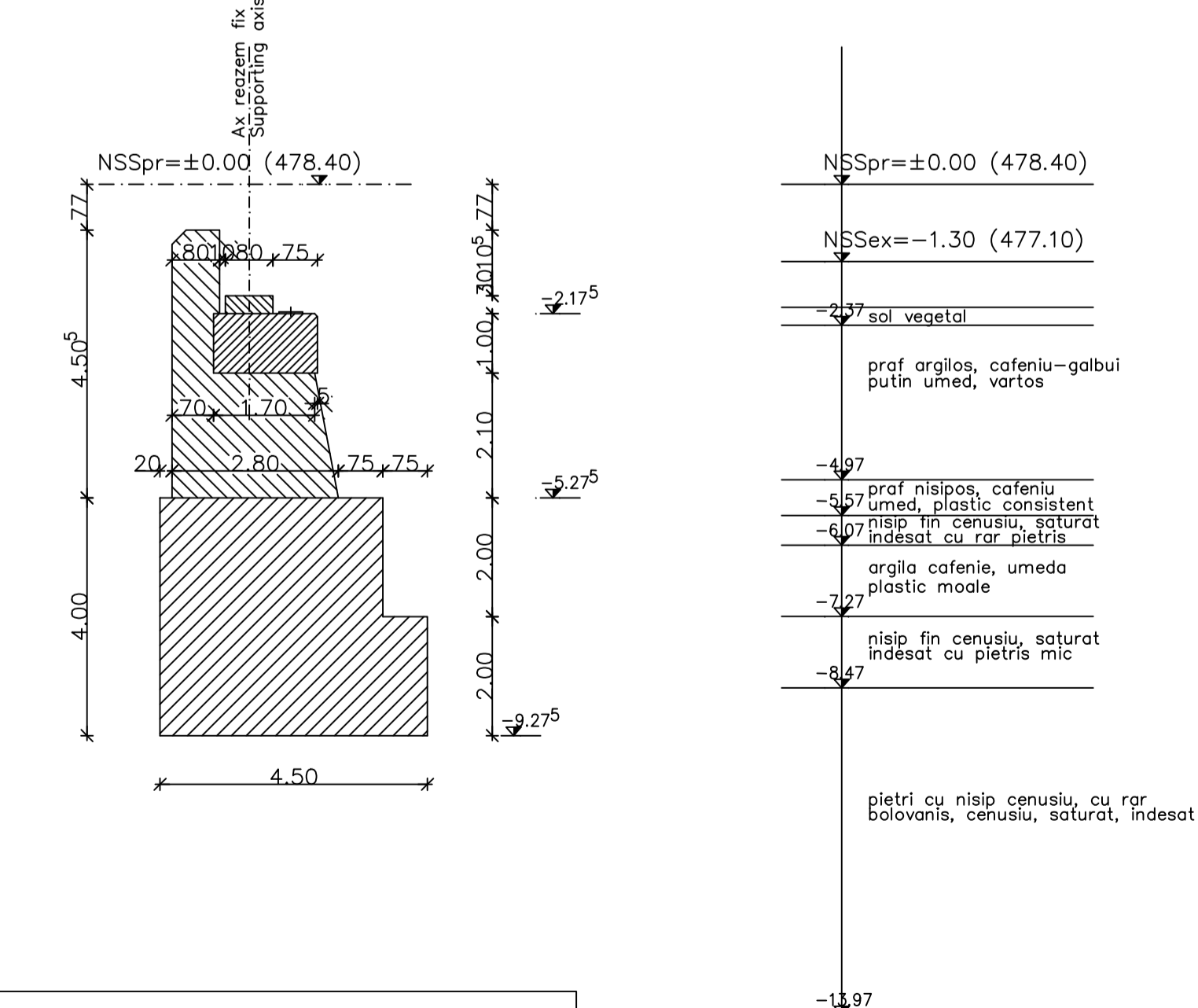
SECTIUNE TRANSVERSALA G-G / G-G CROSS SECTION



SECTIUNE TRANSVERSALA H-H / H-H CROSS SECTION



SECTIUNE TRANSVERSALA I-I / I-I CROSS SECTION



FORAJ/DRILL nr.1F07"  
Km.Ex. 207+308

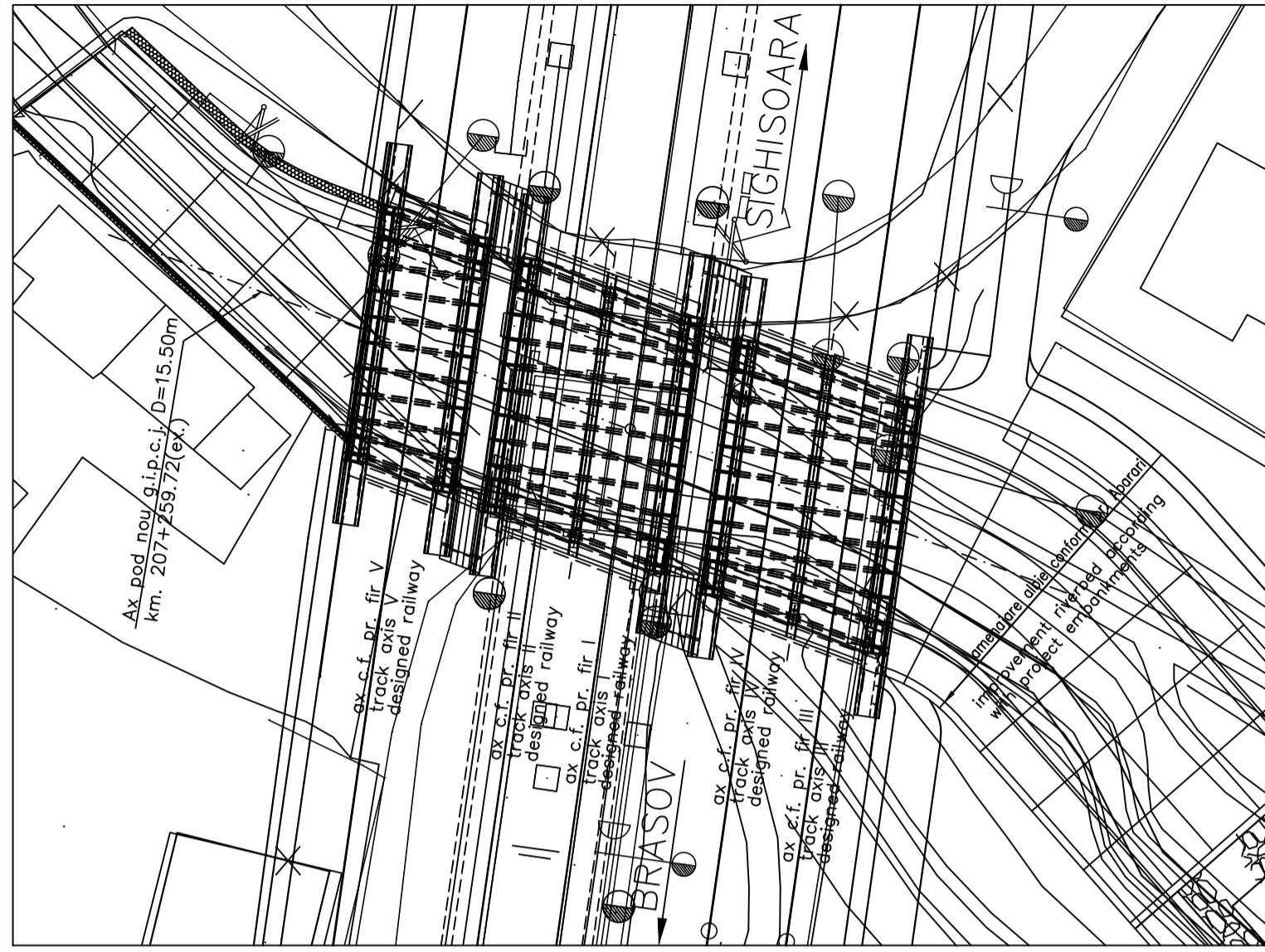
NOTA

- Prezentul plan s-a intocmit pe baza urmatoarelor date:
  - plan de situatie;
  - profil in lung;
  - profil transversal;
  - fisa podului;
  - calculul hidrolic;
  - date culese pe teren.
- Podul corespunde la convooalele de calcul UIC (LM71 si SW/2).
- Noul pod are urmatoarea alcotuire:
  - suprastructura: doua tabiere care dubla si un tablier cale simpla, alcatuite din şip.c.i. cu coala pe balast, L=15,50m
  - infrastructura: culci din beton armat, fundate direct.
- Deaia la executie se vor constata neconcordante intre situatia existenta pe teren si cea din proiect se va anunta proiectantului.
- La executie se vor respecta cu strictete prevederile din "Normativ pentru producerea betonului si executarea lucrarilor din beton, beton armat si beton precomprimat. Partea 1: Producerea betonului", indicativ NE 012/1-2007 si "Normativ pentru producerea betonului si executarea lucrarilor din beton, beton armat si beton precomprimat. Partea 2: Executarea lucrarilor din beton", indicativ NE 012/2-2010, iar verificarea calitatii lucrarilor si receptiunea lor se va face conform normativului C56-1985.
- Construcţia se încadrează în categoria de importanță B (construcții de importanță deosebită), modelul 1 de asigurare a calitatii și clasei de importanță B conform HG 766/1997.
- Proiectul va fi verificat la exigentele A4.2, B2.2, D2.2

NOTE

- This plan has been drawn up based on the following data:
  - Layout Plan;
  - Longitudinal Profile;
  - Cross-section Profile;
  - Bridge's file;
  - Hydraulic determination;
  - Field Data.
- Bridge convooys calculation corresponds to UIC (LM71 and SW/2).
- The new bridge has the following structure:
  - Superstructure: two decks for double track and one deck for single track, plain deck beams, bottom track, with track on ballast, L=15,50m;
  - Infrastructure: abutments and reinforced concrete, with direct foundation.
- The execution will strictly comply with the provisions of "Practice code for concrete production NE 012/1-2007 and "Practice code for the concrete production and works execution, reinforced and pre-stressed concrete - Part 2: The execution of concrete works" NE 012/2-2010, and the quality and the reception of the works shall be made accordingly to Norm C 56-1985.
- In case during the execution there will be found out any discrepancies between the existing situation on site and in project, the designer will be announced.
- The construction is classified in the B category of importance. Special importance constructions, model 1 for quality ensuring according to HG 766/97.
- The project will be checked to comply with the requirements A4.2; B2.2; D2.2.

DATE HIDRAULICE		HIDRAULIC DATA	
Înălțimea de apă	h=1.39m	Înălțimea hidrolică	h=1.39m
Perimetrul udat	P=15.25m	Perimetrul	P=15.25m
Aria udată	A=17.80mp	Suprafața	A=17.80sqm
Viteza de curgere a apei	4.9m/s	Viteza de curgere	V=4.9m/s



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D	C	B	A	Index	Date	Modificari	Proiectant	Proiectant	Proiectant	Proiectant
			11.2011		Revizua 1	1.0	Andrei Costan	Andrei Costan	Andrei Costan	Andrei Costan

GUVERNUL ROMÂNIEI ROMANIAN GOVERNMENT  
PROIECT FINANȚAT DE UNIUNEA EUROPEANĂ EUROPEAN UNION FINANCED PROJECT

CFR  
C.N.C.F. "C.F.R." - S.A.

CLIENT / CLIENT  
TAUFER  
SCOTT WILSON  
OBERMEYER PLANEN + BERATEN GmbH  
TECNIC Consulting Engineers

CONSULTANT / CONSULTANT		Date	Signature
Approved	Self project Project manager	R. Luizza	
Approved	Coordinator Section 1	C. Gambelli	
Checked	Expert Chief	V. Kallidromitis	

SUBCONTRACTANT / SUBCONTRACTOR		Date	Signature
Approved	Responsible Subcontractant Subcontractant Responsible	A. Dinulescu Stanciu	10.2011
Elaborated	Designer	Andrei Costan	10.2011

Reabilitarea liniei de cale ferată Braşov - Simeria, parte componentă a coridorului IV Pan European, pentru circulația trenurilor cu viteză maximă de 160 km/h.  
Tronsoanel: Braşov - Sighisoara  
Rehabilitation of the railway line Braşov - Simeria, component Part of the IV Pan-European Corridor, for the trains circulation with maximum speed of 160 km/h.  
Section: Braşov - Sighisoara

Denumire desen / Drawing Title:  
STATIA / STATION APATA  
POD / BRIDGE Km 207+257.850 - D=15.50m -  
DISPOZITIE GENERALA / GENERAL DISPOSITION

EA51	01	C	09	PA	PV	01	7	0	002	1
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