

NOTA

1. Prezenta plan va fi intocmit in conformitate cu dispozitiile generale.
2. Executia zidurilor intarite si a zidului de garda se va face dupa executia taluzamentului pe mazonerie.
3. La executie se vor respecta cu strictete prevederile din "Normativ pentru producerea betonului si executarea lucrarilor din beton, beton armat si beton precomprimit. Partea I: Producerea betonului", indicativ NE 012/2-2007 si "Normativ pentru producerea betonului si executarea lucrarilor din beton, beton armat si beton precomprimit. Partea 2: Executarea lucrarilor din beton", indicativ NE 012/2-2010, iar mansonarea calitatii lucrarilor si receptiunile lor se va face conform normativului CSE-1995.
4. Constructia se incadreaza in categoria B de importanta. Constructia de importanta deosebita, modelul 1 de asigurare a calitatii conform HG 766/97.
5. Podul corespunde la convoaiazele de calcul UIC (LMT1 si SWQ2).

NOTE:

1. This plan has been drawn up according to the general disposition.
2. The reinforced walls and the guard wall shall be carried out after pulling the desk on the supports.
3. The execution shall strictly comply with the provisions of "Practice code for concrete production and work execution, reinforced and pre-stressed concrete - Part 1: Practice code for concrete production" NE 012/2-2007 and "Practice code for the concrete production and work execution, reinforced and pre-stressed concrete - Part 2: The execution of concrete works" NE 012/2-2010, and the quality and the reception of the works shall be made according to Norm C SE-1995.
4. The construction is classified in the B category of importance. Special importance construction, model 1 for quality ensuring according to HG 766/97.
5. Bridge conveyance calculation corresponds to UIC (LMT1 and SWQ2).

LEGENDA BETOANELOR/CONCRETE LIST

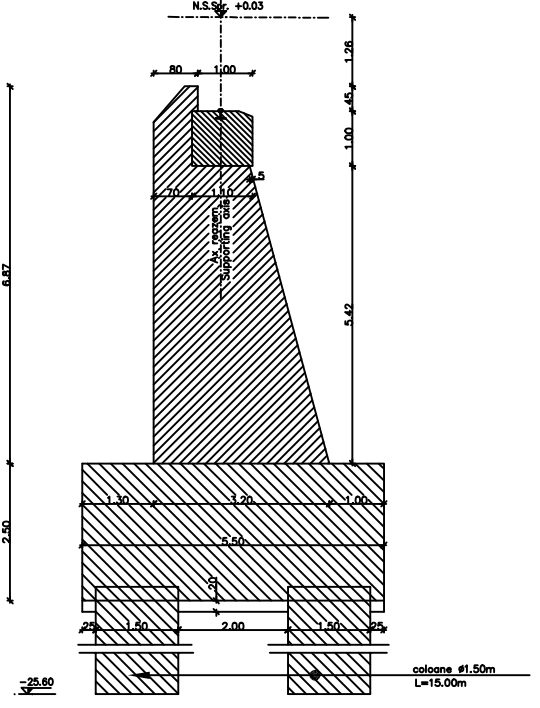
- Beton armat in cuznetii si in bancheta cuznetilor
Reinforced concrete in the cushions and cushion bench
C 35/45-CEM II/A-S 42,5-(XC4+XF3+XA2)-A/C=0,50-D₁₆-Cl 0,20
- Beton armat in elevatiile si in zidurile intarite
Reinforced concrete in the elevations and retained walls
C 25/30-Ciment H II/A-S 42,5-(XC4+XF3)-A/C=0,50-D₁₆-Cl 0,20
- Beton simplu in fundatii
Plain concrete in abutments foundations
C 25/30-Ciment H II/A-S 42,5-(XF3)-A/C=0,50-D₁₆-Cl 0,20

In cazul in care temperatura in timpul turnarii este scazuta, se vor folosi cimenturile cu rezistenta initiala mare, R si aditivi acceleratori, iar in cazul turnarii pe timp cald, cimenturile cu rezistenta initiala usoara, N si aditivi intarziatori (conf. NE 012/2-2010 si tabelul 2 din SR EN 197-1:2002)

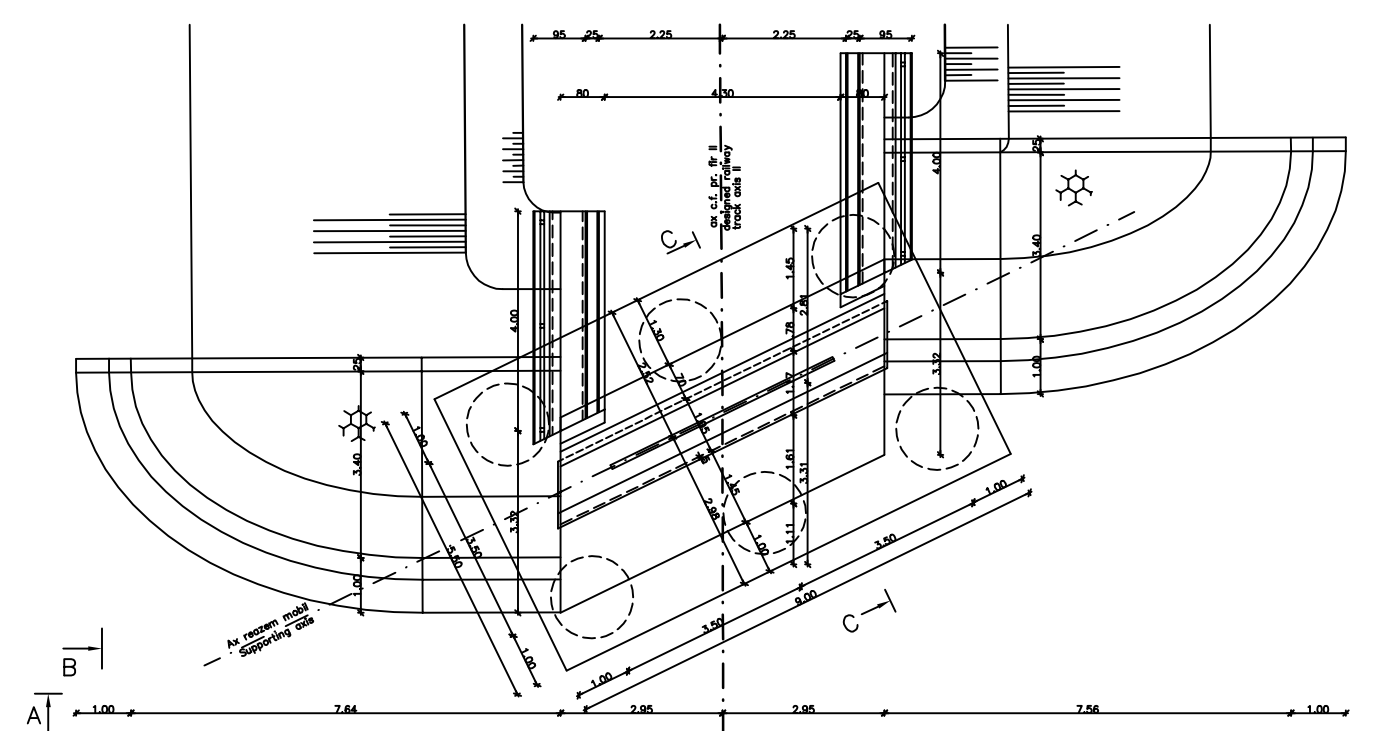
When the temperature during the casting is low, cements with high initial resistance, R and accelerating additives shall be used and when it is cast during warm weather, cements with common initial resistance, N and delaying additives shall be used (according to the norm NE 012/2-2010 and table 2 for the SR EN 197-1: 2002).

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SECTIUNE TRANSVERSALA C-C
C-C CROSS SECTION



VEDERE IN PLAN/ PLAN VIEW



D									
C									
B									
A	12.2011	Revizua 1 i Revizua	Andrei Costan	Arhitect	Arhitect	Arhitect	Arhitect	Arhitect	Arhitect
Indice	Date	Modificari	Desenator	Proiectant	Arhitect	Arhitect	Arhitect	Arhitect	Arhitect
<p>GUVERNUL ROMANIEI ROMANIAN GOVERNMENT</p> <p>PROIECT FINANȚAT DE UNIUNEA EUROPEANĂ A EUROPEAN UNION FINANCED PROJECT</p> <p>CLIENT / CLIENT: C.N.C.F. "C.F.R." - S.A.</p> <p>CONSULTANT / CONSULTANT: OBERMEYER, TECNIC</p> <p>SUBCONTRACTANT / SUBCONTRACTOR: A. Ditușescu Stancu</p> <p>Reabilitarea liniei de cale ferată Brașov - Simleu, parte componentă a a coridorului IV Pan European, pentru circulația trenurilor cu viteză maximă de 160 km/h, Tronsoanel: Brașov - Slighșoara Rehabilitation of the railway line Brașov - Simleu, component part of the IV Pan-European Corridor, for the trains circulation with maximum speed of 160 km/h, Section: Brașov - Slighșoara</p> <p>Denumire desen / Drawing Title: INTERVAL RACOS - CATA Pod/Bridge Km231+746,888 - D=12,00m PLAN COFRAJ CULEEA BRASOV LINIA 2 ABUTMENT BRASOV TRACK 2 FORMWORK DRAWING</p> <p>Codificare / Codification System: Scale / Scale: 1:50, LOT / LOT: No. / No. 01/01</p>									