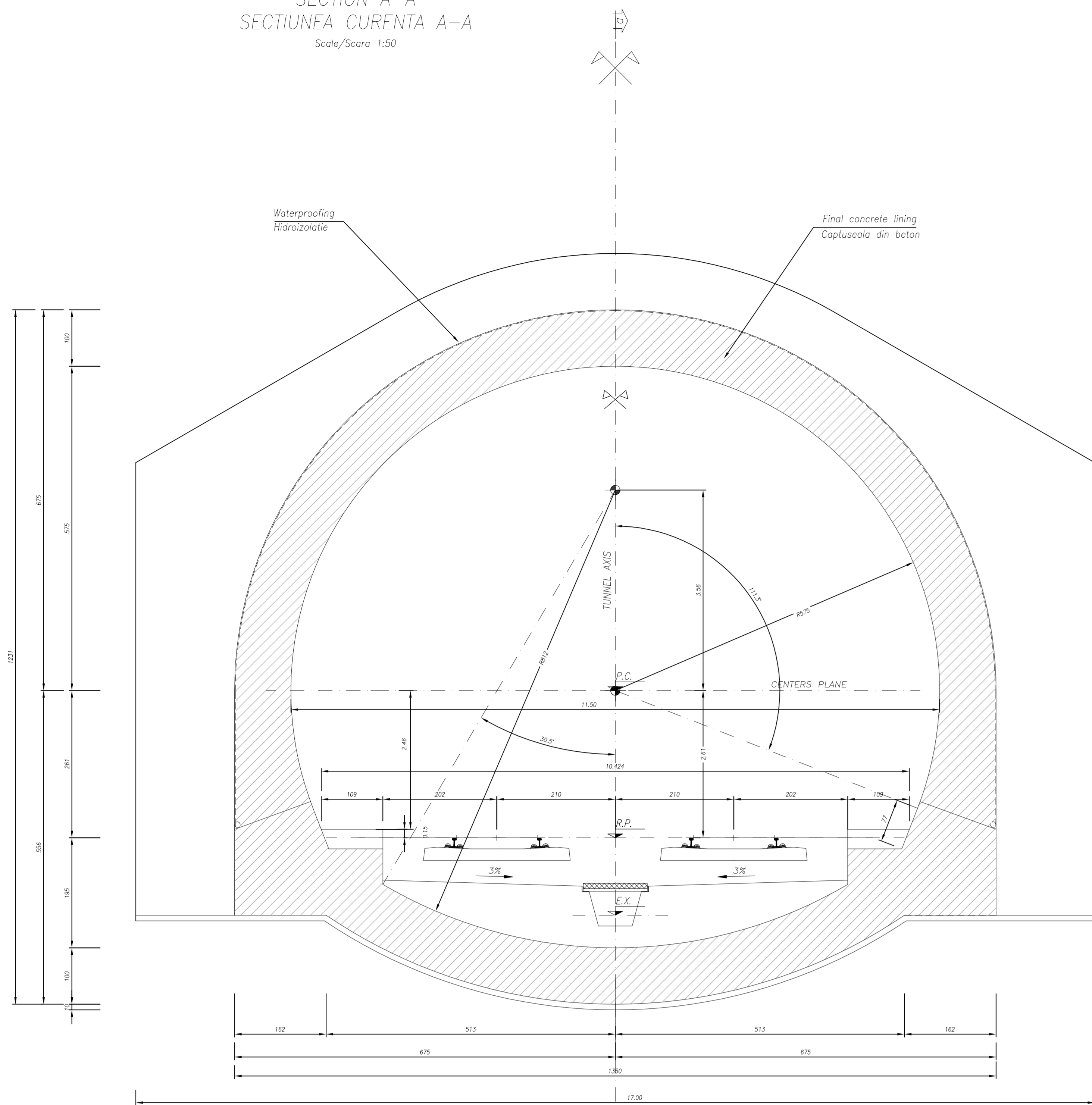
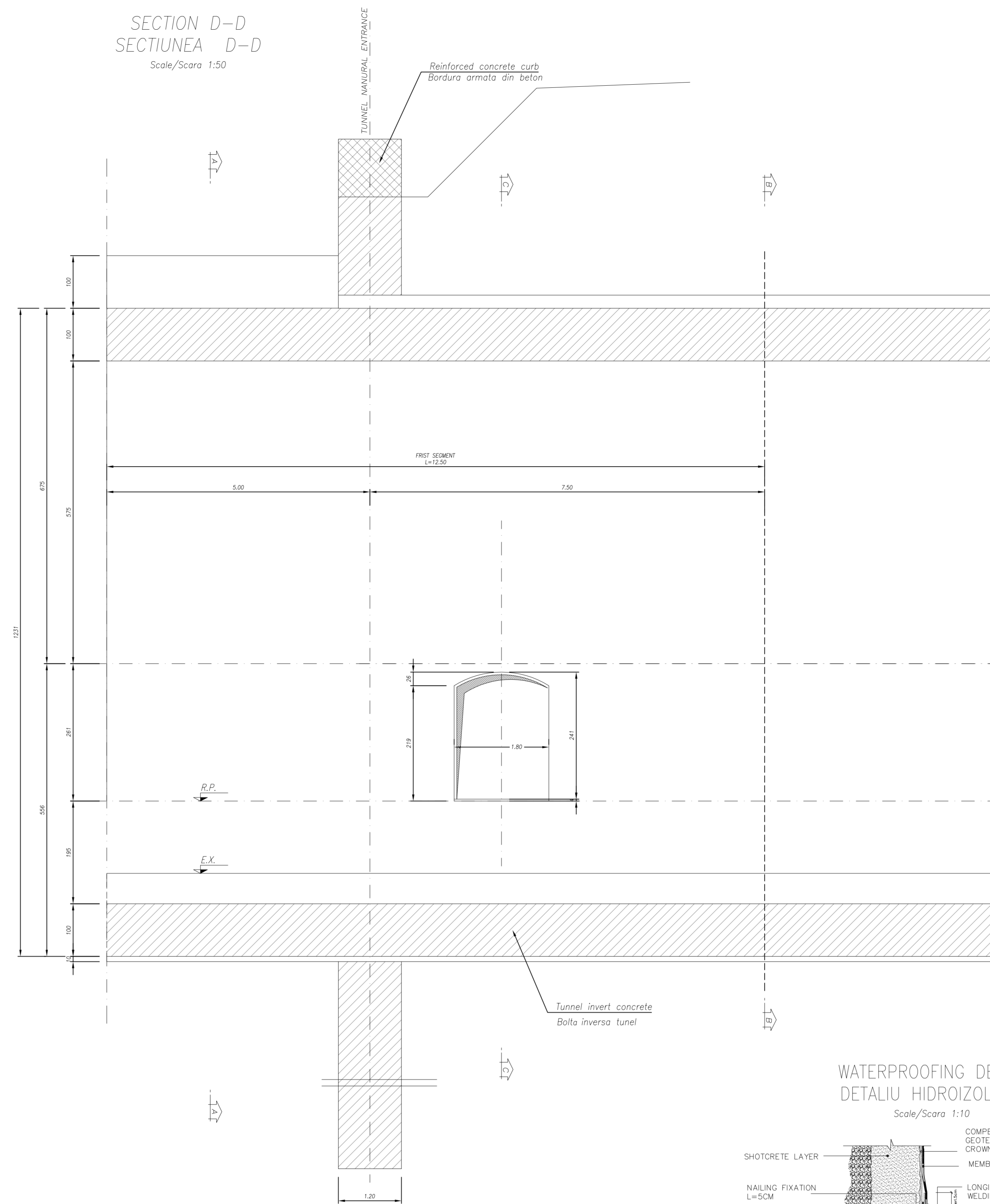


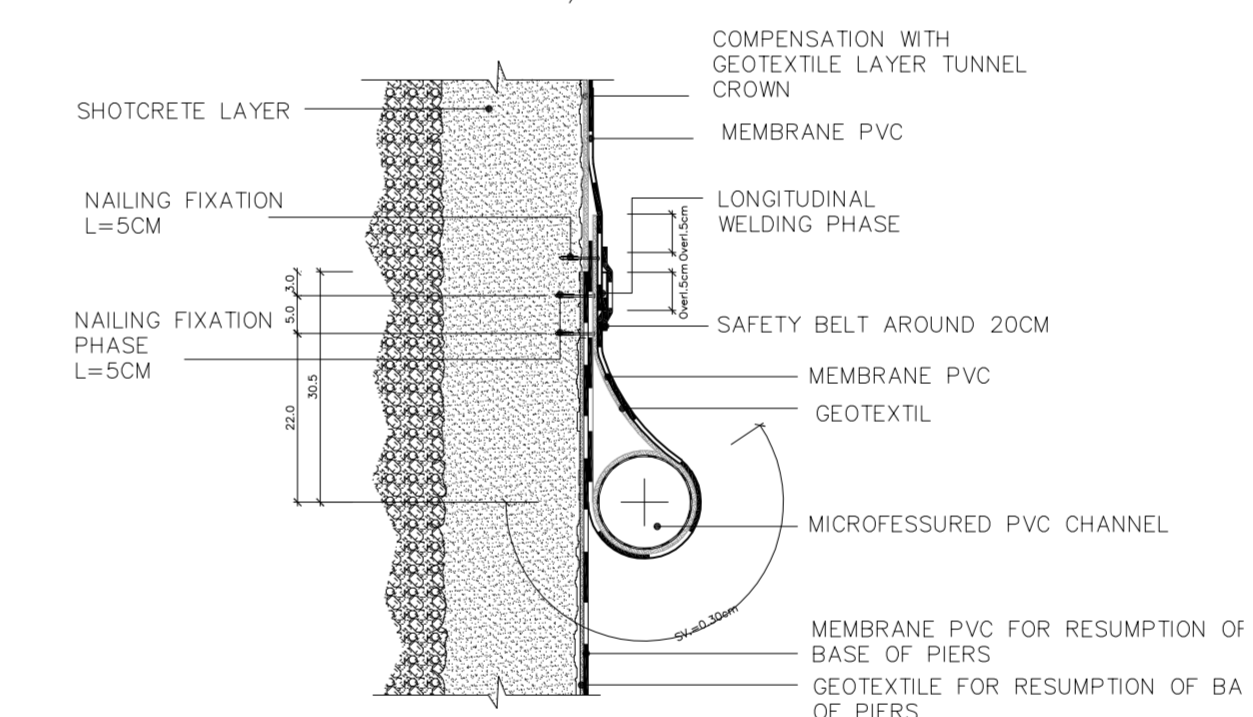
SECTION A-A
SECTIUNEA CURENTA A-A
Scale/Scara 1:50



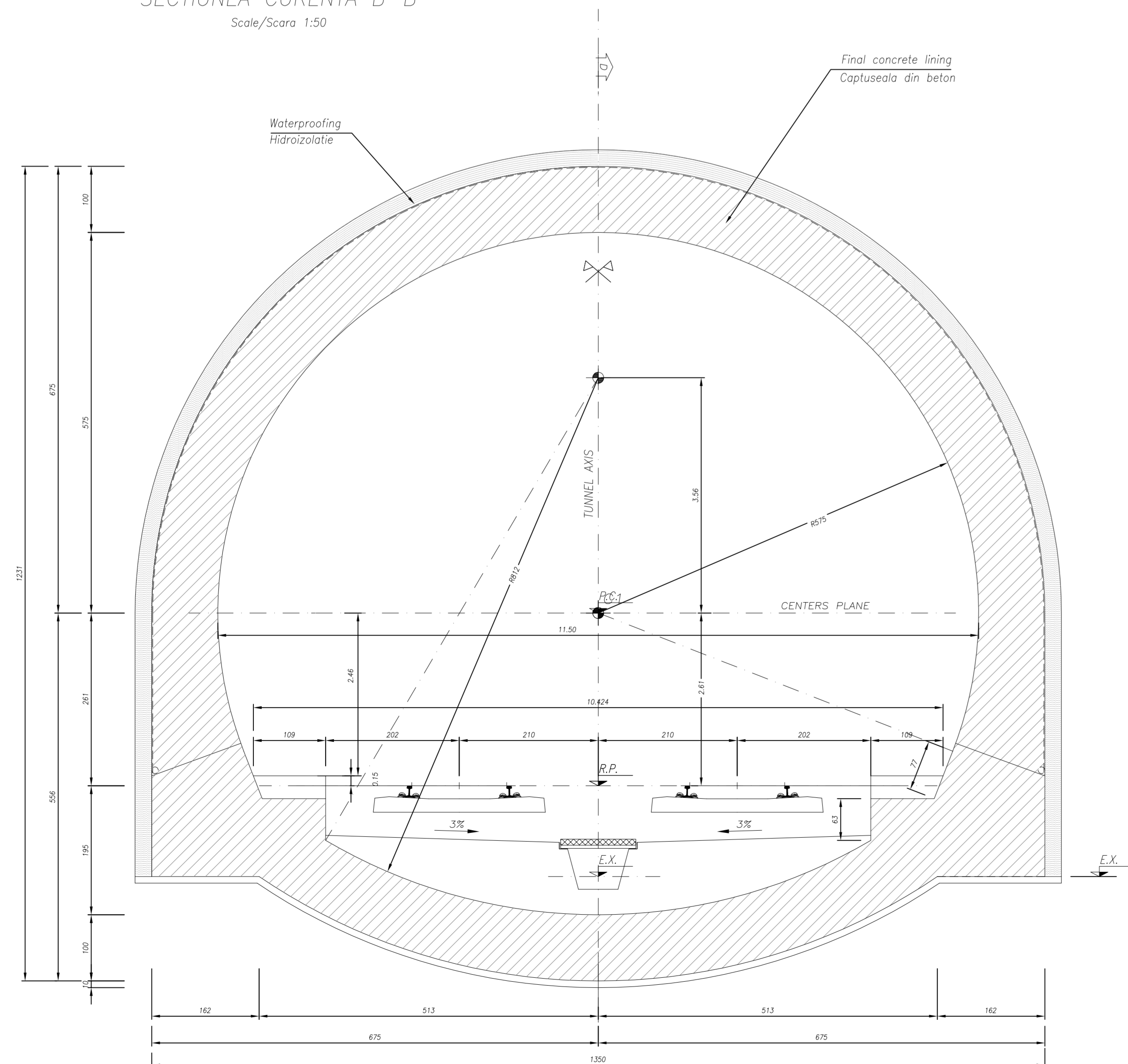
SECTION D-D
SECTIUNEA D-D
Scale/Scara 1:50



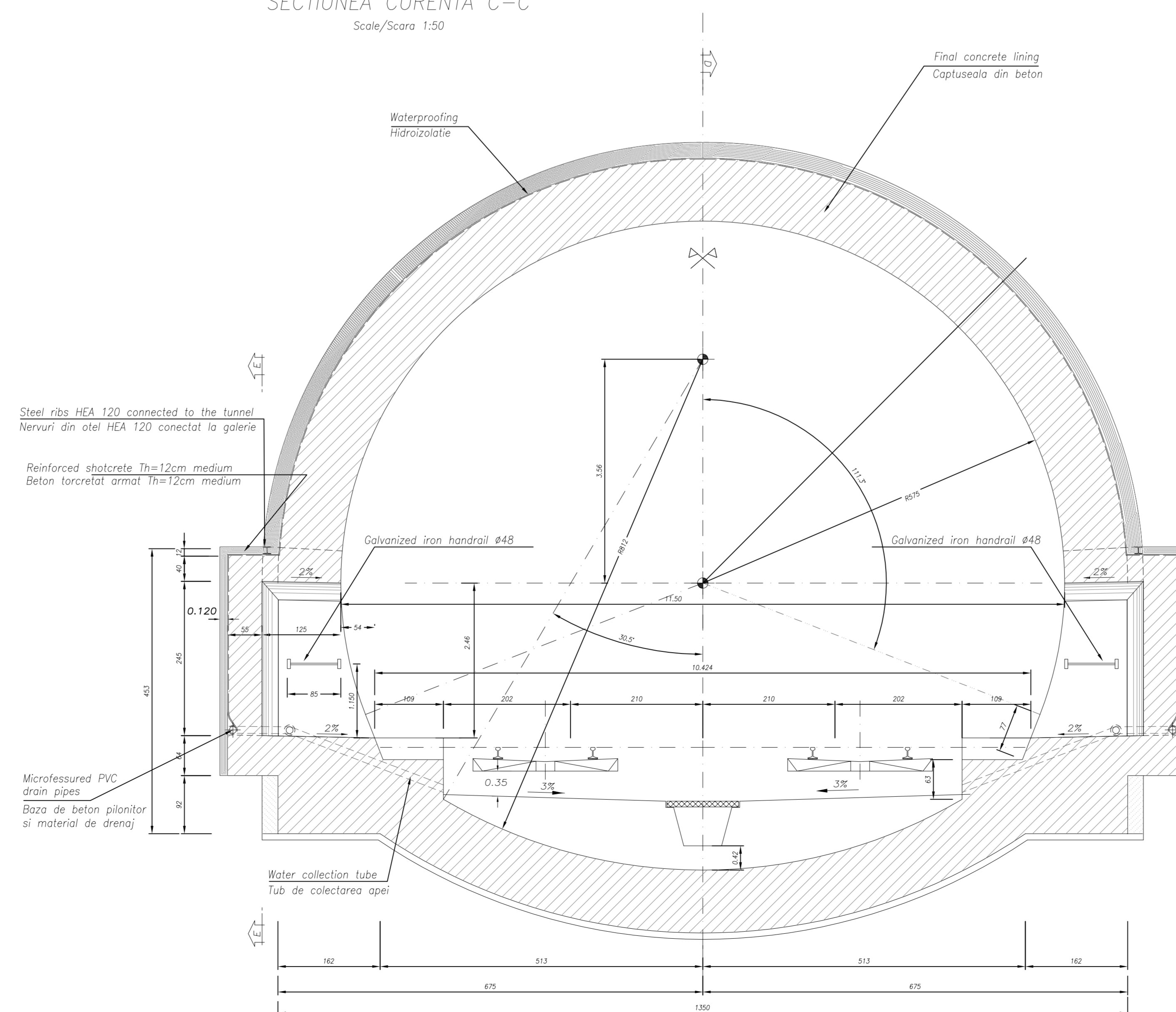
WATERPROOFING DETAIL
DETALIU HIDROIZOLATIE
Scale/Scara 1:10



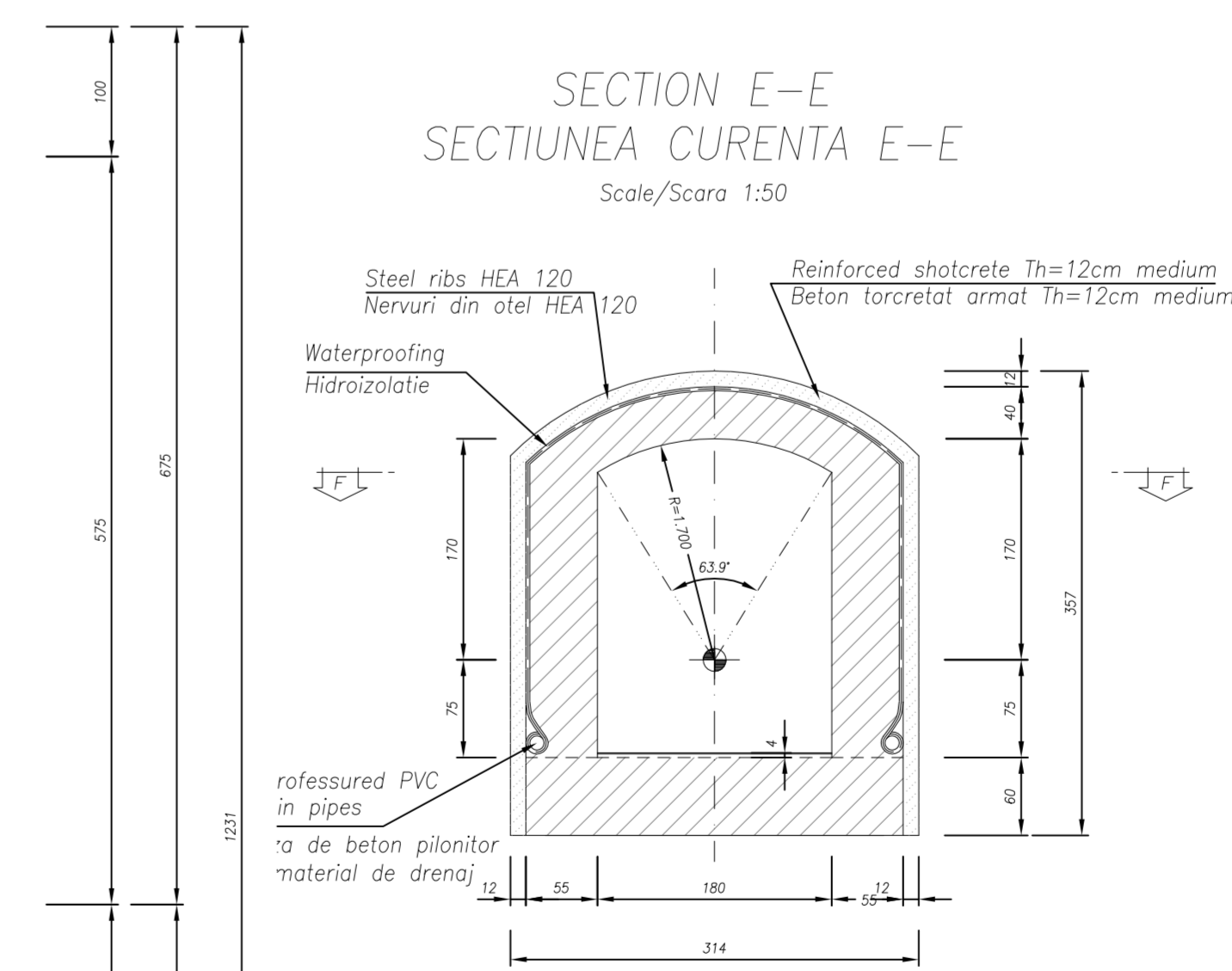
SECTION B-B
SECTIUNEA CURENTA B-B
Scale/Scara 1:50



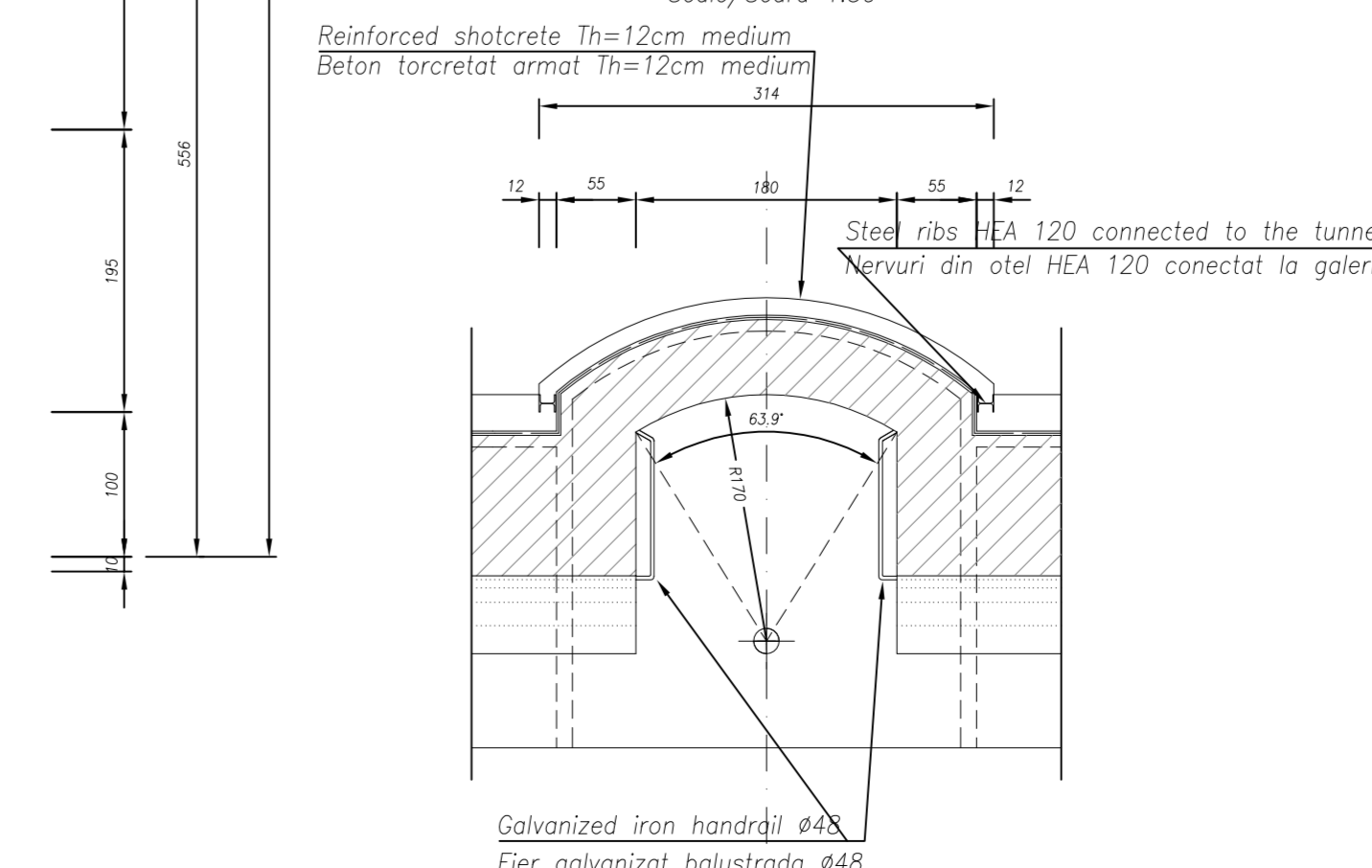
SECTION C-C
SECTIUNEA CURENTA C-C
Scale/Scara 1:50



SECTION E-E
SECTIUNEA CURENTA E-E
Scale/Scara 1:50



SECTION F-F
SECTIUNEA CURENTA F-F
Scale/Scara 1:50



MATERIAL TABLE

TUNNEL INVERT CONCRETE:

- Type C2
- fc_k >=30 MPa
- Water/cement ratio <0.50
- Minimum structural Class S3/S4
- Exposure class related to environmental conditions: XA2
- Cement type CEM III/V
- Class of chloride content Cl 0.20
- # max aggregate = 32 mm

TUNNEL CROWN CONCRETE:

- Type C1
- fc_k >=30 MPa
- Water/cement ratio <0.50
- Minimum structural Class S4/S5
- Exposure class related to environmental conditions: XA2
- Cement type CEM III/V
- Class of chloride content Cl 0.20
- # max aggregate = 32 mm

REINFORCING STEEL:

- B450C controlled by establishment
- Weldable

LEAN CONCRETE:

- Type I
- fc_k >=12 MPa
- Cement type CEM I/V

LEGEND

RP = REFERENCE PLANE
EX = EXCAVATIONS PLANE
PC = CENTERS PLANE

NOTE:

The integrity of the concrete structures of the artificial and natural tunnels have to be maintained in case of fire (4.2.2.3 Fire protection requirements for structure TS1 Technical Specification for interoperability).

NOTA:

Integritatea structurilor de beton a tunelului artificial si naturale, trebuie sa ramana intacta in caz de incendiu (4.2.2.3 cerintele de protectie la foc pentru structuri S71 Specificatii tehnice pentru interoperabilitate).

NOTE:

Handrail shall be installed approximately at 1 m above walkway providing a route to a safe area (4.2.2.7. Escape walkways TS1).

NOTA:

Bara de mana trebuie sa fie instalata aproximativ la 1 m deasupra caili de evacuare oferind un traseu intr-o zona sigura (4.2.2.7 evacuare tratate S71).

NOTE:

Escape signs shall be installed on the side walls at a maximum distance between escape signs of 50m (4.2.2.9 Escape Signage TS1).

NOTA:

Semnele de evacuare trebuie sa fie instalate pe peretii laterali, la o distanta maxima intre semne de evacuare de 50m (4.2.2.9 Escape Signage S71).

D						
C						
B						
A						

Index	Date	Modificari	Proiectant	Approbat	Approbat CFR
		Modificator/Revisi	Designer	Approved Consultant	Approved CFR

GUVERNUL ROMANIEI
ROMANIAN GOVERNMENT

PROIECT FINANAT DE UNIUNEA EUROPEANA
EUROPEAN UNION FINANCED PROJECT

CLIENT / CLIENT

C.N.C.F. "C.F.R." - S.A.

CONSULTANT / CONSULTANT		Date	Signature
Approved	Project manager	R. Liuzza	
Approved	Coordinator	C. Gambelli	
Verified	Tunnel Expert	C. Gambelli	
Checked	Tunnel Expert	C. Gambelli	
Elaborated	Designer	C. Gambelli	

SUBCONSULTANT / SUBCONSULTANT

Reabilitarea liniei de cale ferata Brasov - Simeria, parte componenta a coridorului IV Pan European, pentru circulatia trenurilor cu viteza maxima de 160 km/h. Sectiunea 1 Brasov - Sighisora

Rehabilitation of the railway line Brasov - Simeria, component Part of the IV Pan-European Corridor, for the trains circulation with maximum speed of 160 km/h. Section 1 Brasov - Sighisora

Denumire desen / Drawing Title: **ARCHITA 2 TUNNEL**
ENTRANCE ARCHITA SIDE
First segment: formworks
Primum segment: cofraje de constructie

Codificare / Codification System	Scara / Scale	LOT	Nr. / No
E A 5 1	0 1 C 1 4 B B	G A 0 9 0 0	0 0 4 0